



# WOKINGHAM BOROUGH COUNCIL

A Meeting of the **PLANNING COMMITTEE** will be held  
David Hicks 1 - Civic Offices, Shute End, Wokingham RG40  
1BN on **WEDNESDAY 12 JULY 2017 AT 7.00 PM**

A handwritten signature in black ink, appearing to read 'Andy Couldrick'.

Andy Couldrick  
Chief Executive  
Published on 4 July 2017

This meeting will be filmed for inclusion on the Council's website.

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# **WOKINGHAM BOROUGH COUNCIL**

## **Our Vision**

A great place to live, an even better place to do business

## **Our Priorities**

Improve educational attainment and focus on every child achieving their potential

Invest in regenerating towns and villages, support social and economic prosperity, whilst encouraging business growth

Ensure strong sustainable communities that are vibrant and supported by well designed development

Tackle traffic congestion in specific areas of the Borough

Improve the customer experience when accessing Council services

## **The Underpinning Principles**

Offer excellent value for your Council Tax

Provide affordable homes

Look after the vulnerable

Improve health, wellbeing and quality of life

Maintain and improve the waste collection, recycling and fuel efficiency

Deliver quality in all that we do

## MEMBERSHIP OF THE PLANNING COMMITTEE

### Councillors

Tim Holton (Chairman)	John Kaiser (Vice-Chairman)	Philip Houldsworth
John Jarvis	Malcolm Richards	Angus Ross
Rachelle Shepherd-DuBey	Wayne Smith	Bill Soane

ITEM NO.	WARD	SUBJECT	PAGE NO.
12.		<b>APOLOGIES</b> To receive any apologies for absence.	
13.		<b>MINUTES OF PREVIOUS MEETING</b> To confirm the Minutes of the Meeting held on 14 June 2017	5 - 10
14.		<b>DECLARATION OF INTEREST</b> To receive any declaration of interest	
15.		<b>APPLICATIONS TO BE DEFERRED AND WITHDRAWN ITEMS</b> To consider any recommendations to defer applications from the schedule and to note any applications that may have been withdrawn.	
16.	Wescott	<b>APPLICATION NO 170264 - LAND EAST OF FINCHAMPSTEAD ROAD, FINCHAMPSTEAD NORTH, WOKINGHAM, RG40 3HE</b> Recommendation: Approval	11 - 34
17.	Shinfield South	<b>APPLICATION NO 170010 - LAND NORTH OF CROFT ROAD, SPENCERS WOOD, RG7 1DR</b> Recommendation: Approval	35 - 66
18.	Emmbrook	<b>APPLICATION NO 170618 - MATTHEWSGREEN PHASE 3</b> Recommendation: Approval	67 - 90
19.	Arborfield; Barkham; Finchampstead South; Swallowfield	<b>APPLICATION NO 170686 - PARCELS H-J, ARBORFIELD GARRISON</b> Recommendation: Approval	91 - 118
20.	Remenham, Wargrave and Ruscombe	<b>APPLICATION NO 171328 - 15-27 HIGH STREET, WARGRAVE, RG10 8BU</b> Recommendation: Approval	119 - 146

- |     |                 |  |           |
|-----|-----------------|--|-----------|
| 21. | Shinfield North | <b>APPLICATION NO 171436 - LAND ADJACENT TO 1 ANSON WALK, SHINFIELD, RG2 8JU</b><br><b>Recommendation:</b> Approval                | 147 - 170 |
| 22. | Sonning         | <b>APPLICATION NO 170580 - OLD REDINGENSIANS SPORTS GROUND, OLD BATH ROAD, SONNING, RG4 6TQ</b><br><b>Recommendation:</b> Approval | 171 - 196 |

**Any other items which the Chairman decides are urgent**

A Supplementary Agenda will be issued by the Chief Executive if there are any other items to consider under this heading.

**GLOSSARY OF TERMS**

The following abbreviations were used in the above Index and in reports.

<b>C/A</b>	Conditional Approval (grant planning permission)
<b>CIL</b>	Community Infrastructure Levy
<b>R</b>	Refuse (planning permission)
<b>LB</b>	(application for) Listed Building Consent
<b>S106</b>	Section 106 legal agreement between Council and applicant in accordance with the Town and Country Planning Act 1990
<b>F</b>	(application for) Full Planning Permission
<b>MU</b>	Members' Update circulated at the meeting
<b>RM</b>	Reserved Matters not approved when Outline Permission previously granted
<b>VAR</b>	Variation of a condition/conditions attached to a previous approval
<b>PS</b>	Performance Statistic Code for the Planning Application
<b>Category</b>	

**CONTACT OFFICER**

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**MINUTES OF A MEETING OF THE  
PLANNING COMMITTEE  
HELD ON 14 JUNE 2017 FROM 7.00 PM TO 8.55 PM**

**Committee Members Present**

Councillors: Tim Holton (Chairman), John Kaiser (Vice-Chairman), John Jarvis, Malcolm Richards, Angus Ross, Rachelle Shepherd-DuBey, Wayne Smith and Bill Soane

**Other Councillors Present**

Councillors: David Lee and Barrie Patman

**Officers Present**

Chris Easton, Service Manager, Highways Development Management  
Mary Severin, Borough Solicitor  
Justin Turvey, Operational Development Management Lead Officer  
Arabella Yandle, Democratic Services Officer

**Case Officers Present**

Katie Herrington, Senior Planning Officer  
Pooja Kumar, Senior Planning Officer  
Ashley Smith, Development Management Team Manager  
Graham Vaughan, Senior Planning Officer  
Christopher Kempster, Career Grade Planning Officer  
Daniel Ray, Senior Planning Officer

**1. APOLOGIES**

An apology for absence was submitted from Councillor Philip Houldsworth

**2. DECLARATION OF INTEREST**

Councillor John Jarvis declared a personal and prejudicial interest in Item 11, Application no 171120 – Grovelands Park, Grovelands Avenue because he was a Non-Executive Director on Wokingham Housing Ltd at the time that the application was submitted. (He left the meeting during consideration of the matter and did not take part in the vote.)

Councillor Angus Ross declared a personal interest in items 5 and 10: Application no 170264 – Land at Finchampstead Road; and Application no – 171143 – Ryeish Sports Centre, stating that though he had been the Executive Member for the Environment when these applications had been submitted, he still had an open mind with regard to the decision.

**3. MINUTES OF PREVIOUS MEETING**

The Minutes of the meeting of the Committee held on 10 May 2017 were confirmed as a correct record and signed by the Chairman.

**MEMBERS' UPDATE**

There are a number of references to the Members' Update within these minutes. The Members' Update was circulated to all present prior to the meeting. A copy is attached.

**4. APPLICATIONS TO BE DEFERRED AND WITHDRAWN ITEMS**

No applications were recommended for deferral or withdrawn

## **5. APPLICATION NO 170264 - LAND AT FINCHAMPSTEAD ROAD**

*(Councillor Angus Ross declared a personal interest in this item)*

**Proposal:** Full application for the proposed change of use of land to Suitable Alternative natural greenspace (SANG) with associated landscape works and parking.

**Applicant:** Gladman Developments

The Committee received and reviewed a report about this application, set out in Agenda pages 13 to 30.

The Committee were advised that the Members' Update included:

- Clarification that the application stood alone and unrelated to any development;
- Confirmation that the WBC Drainage Officer was satisfied with the scheme;
- Proposed amendment to Condition 2, and
- Proposed update to plan numbers in Condition 4.

Clarissa Flynn, Residents Group, spoke against the application. With the aid of a presentation, she suggested that the proposed site for the SANG was not an ideal one. The existing path and access were narrow, there was no bus access and the site was prone to flooding. A tree survey had indicated that eight veteran oaks with Tree Protection Orders would be felled. She stated that there were many other sites that were better suited to development as SANGs and suggested that the application would not have been proposed without the intention to build houses nearby.

David Lee, Ward Member for Norreys, read a statement on behalf of Julian Mcghee-Sumner, Ward Member for Westcott, in opposition to the application. He stated that the application was premature and that it was intrinsically linked to a development.

In response, the Case Officer stated that the application before the Committee was a separate matter from any future application pertaining to development. He indicated that the SANG would use existing access so would have little impact on trees. The WBC officer was satisfied with the SANG application.

Following discussion, Councillor John Kaiser proposed that the application be deferred to allow Members to attend a site visit to gain a fuller understanding of the application. This was seconded by Councillor Rachelle Shepherd-DuBey.

**Resolved:** That Application no 170264 be deferred in order to allow the Committee to undertake a site visit.

## **6. APPLICATION NO 162885 - READING FOOTBALL CLUB AT FOREST SCHOOL**

**Proposal:** Full planning application for indoor sports facility; means of access and parking; ancillary outbuildings and associated landscaping.

**Applicant:** Reading Football Club

The Committee received and reviewed a report about this application, set out in Agenda pages 31 to 62.

The Committee were advised that the Members' Update included:

- Clarification that Forest School was an academy;
- Clarification as to construction hours, and
- A proposed update to condition 3 listing amended plan numbers.

Nina Sharp, Consultant and Agent for Barton Wilmore, spoke in favour of the application, outlining the close working relationship between Reading Football Club and the Forest School. She stated that benefits to the proposals included improved playing field surfaces and landscaping, together with an increase in community use.

In response to a Member question regarding lighting in the dome, she explained that the dome structure reflected light into the interior only and would not therefore create any light pollution.

**Resolved:** That Application no 162885 be approved, subject to the conditions set out in Agenda pages 31 to 62, and the update to Condition 3 and the clarifications as laid out in the Members' Update.

## **7. APPLICATION NO 161911 - CROSFIELDS SCHOOL, SHINFIELD ROAD**

**Proposal:** Full application for the proposed artificial turf pitch with fencing and floodlighting, re-grading of land for two new football pitches and an artificial cricket strip

**Applicant:** Crosfields School, Shinfield Road, Wokingham

The Committee received and reviewed a report about this application, set out in Agenda pages 63 to 92.

Neil Boddington, Agent, spoke in favour of the application, stating that he had worked closely with the Case Officer on the application. The application included a bio-diversity condition, which the applicants are happy to implement and work with the Council on. There was no intention to develop the land in future.

**Resolved:** That Application no 161911 be approved subject to the conditions set out in Agenda pages 63 to 92.

## **8. APPLICATION NO 163372 - WOKINGHAM THEATRE, TWYFORD ROAD**

**Proposal:** Full application for the proposed internal/external alterations to theatre to provide additional backstage space, rehearsal space, new studio, public circulation areas and new storage barn.

**Applicant:** Wokingham Theatre Board

The Committee received and reviewed a report about this application, set out in Agenda pages 93 to 118.

The Committee were advised that the Members' Update included:

- Clarification on the nature of community use provided;
- Proposed amendment to Condition 8, to wit that drainage requires 40% allowance for climate change;
- Proposed amendments to Condition 2 and 4;
- Proposed amendments to plans, and

- Proposed additional condition requiring the submission and approval of a Construction Method Statement prior to any development.

Hedda Bird spoke in favour of the application, outlining the success of the Theatre and the opportunities that expansion would offer, including higher attendance figures, clubs and training. Katie Hamilton then went on to re-iterate the wide community use by local people from eight to 80 years of age.

David Lee, Ward Member for Norreys, spoke in favour of the application, stating that since the original decision in the 1980s to build the facility, it had gone from strength to strength. He asked if there would be an extension granted if part of the expansion had not been completed prior to the end of the 3-year construction condition.

In response, the Case Officer stated that the 3-year construction condition referred to the commencement of development. She also clarified that fire prevention strategies were governed by Building Control regulations and did not come under the purview of the Planning Department.

In response to Member questions regarding the plans and parking, the Service Manager, Highways Development Management, clarified that the regulations required 1 car park space to every 5 seats and that the scheme provided in excess of this number for the additional elements of the proposal.

In response to Member questions regarding the Community Plan, the Lead Officer, Operational Development Management, stated that Ward Members would be consulted on the community use Management Plan in Condition 4.

**Resolved:** That Application no 163372 be approved subject to the conditions set out in Agenda pages 93-118, the amendments to conditions 2 and 8 and the additional condition and clarifications as laid out in the Members' Update, and the amendment to condition 4 as laid out in the Members' Update with an additional comment, to wit that the Community Plan be agreed in conjunction with the Ward Members.

## **9. APPLICATION NO 171143 - RYEISH SPORTS CENTRE** *(Councillor Angus Ross declared a personal interest in this item)*

**Proposal:** Full application for the proposed construction of new sports changing facilities and fitness suite following demolition of the existing building on site.

**Applicant:** Mr Andy Glencross, Wokingham Borough Council

The Committee received and reviewed a report about this application, set out in Agenda pages 143 to 168.

The Committee were advised that the Members' Update included clarifications as to the applicant and the number of car parking spaces provided.

Neil Jones, Senior Operational Property Specialist, spoke in favour of the application, outlining the history of the site and the range of improvements that were being offered by the application.

Barrie Patman, Ward Member for Shinfield South, spoke in favour of the application, stating that the improvements to the site and the wide range of sports and activities that were proposed would increase the footfall. He suggested that access would be improved with good signage and that the site needed to be well advertised.

In response, the Service Manager, Highways Development Management, outlined the road improvements, cycle lanes and the upgrading of footpaths that were taking place in association with wider development that was progressing as part of the South of the M4 Strategic Development Location.

**Resolved:** That Application no 171143 be approved, subject to the conditions set out in Agenda pages 143 to 168 and the clarifications as laid out in the Members' Update.

#### **10. APPLICATION NO 171007 - HIGHWOOD PRIMARY SCHOOL, FAIRWATER DRIVE**

**Proposal:** A full application for a single Storey extension for the creation of six classrooms to the north side of the existing car park and, a new Speech and Language building adjacent to the existing hall to allow for a 2 form entry school. Additional staff parking bays and Cycle Canopy provision. New pedestrian route from Woodlands Avenue and a new reception classroom garden to the north of existing nursery/reception block.

**Applicant:** Wokingham Borough Council

The Committee received and reviewed a report about this application, set out in Agenda pages 119 to 142.

Anju Sharma, Specialist, Growth & Delivery, spoke in favour of the application, stating that, as part of the Primary Schools Strategy, Woodley had been identified as requiring an extra 45 places per year and that the application before the Committee would provide 30 of these. The site was close to new developments that would see approximately 1000 new properties built in the area. The application would provide enhanced facilities, more parking and a specialist unit for speech and language therapy.

In response to Member queries regarding parking, the Service Manager, Highways Development Management, explained that the amount of spaces provided by the proposal was in excess of those required by the Council's parking standards of one space per each Full Time Equivalent (FTE) staff member. The increase in numbers would be phased in over several years, allowing the school to work with the community and to upgrade/develop further the School Travel Plan.

In response to a Member question regarding loss of space, the Case Officer stated that most of the area that was due to be developed was already hard standing.

**Resolved:** That Application no 171007 be approved subject to the conditions set out in Agenda pages 119 to 142.

#### **11. APPLICATION NO 171120 - GROVELANDS PARK, GROVELANDS AVENUE** *(Councillor John Jarvis declared a personal and prejudicial interest in this item)*

**Proposal:** Application to vary conditions 2, 3 and 9 of planning consent F/2014/2611 for the erection of residential development of 6no two bed dwellings with associated external

works. Condition 2, 3 and 9 relate to materials, elevation and boundary treatments respectively

**Applicant:** Wokingham Housing Limited

The Committee received and reviewed a report about this application, set out in Agenda pages 169 to 182.

**Resolved:** That Application no 171120 be approved, subject to the conditions set out in Agenda pages 169 to 182.

# Agenda Item 16.

<b>Development Management Ref No</b>	<b>No weeks on day of committee</b>	<b>Parish of</b>	<b>Ward</b>	<b>Listed by:</b>
170264	24/24	Wokingham	Wescott	Scheme of Delegation

**Applicant Location** Gladman Developments  
Land East Of Finchampstead Road, **Postcode** RG40 3HE  
Wokingham

**Proposal** Full application for the proposed change of use of land to Suitable Alternative natural greenspace (SANG) with associated landscape works and parking.

**Type** Full  
**PS Category** 6 – Major (All other Developments)  
**Officer** Ashley Smith

**FOR CONSIDERATION BY** Planning Committee on 12/07/17  
**REPORT PREPARED BY** Head of Development Management and Regulatory Services

## ADDENDUM REPORT

The application was deferred from Committee in June to allow for a site visit. The site visit has been scheduled for a date prior to the July committee date.

A number of matters were discussed at the June committee and these are addressed below.

### Prematurity:

1. At the June meeting it was suggested that the SANG application is premature. For the reasons outlined in the report, the application is considered by officers to conform with the Development Plan. If this application is found to be consistent with the Development Plan, it cannot be considered premature.

### Update to Preliminary Matters

2. An outline application has been received on the adjacent land for 250 houses. This application has yet to be determined and has the reference 170261. The housing application in particular has received a significant amount of objection. This SANG application is a separate planning application and must be determined on its own merits.
3. There is no certainty on the timeframe that the housing application (170261) will be determined as significant new information (relevant to that application only) is expected shortly and this will be subject to further public consultation.
4. The National Planning Policy Framework stresses the importance of having a planning system that is genuinely plan-led. Where a proposal accords with an up-to-date development plan it should be approved without delay, as required by the

presumption in favour of sustainable development at paragraph 14 of the National Planning Policy Framework.

5. With this in mind the applicant has respectfully requested that a decision be made on this separate SANG application without further delay.

**Letter of the Wokingham Veteran Tree Association:**

6. At the June Planning Committee meeting the comments of the Wokingham Veteran Tree Association were discussed. The WBC Tree officer has reviewed the letter from the association and considers that no unacceptable risk arises with regards to veteran trees. The SANG proposal continues to utilise the same access point on to Finchampstead Road and as such the trees fronting on to Finchampstead Road are unaffected. The Tree and Landscape officer has confirmed that they are satisfied that the field access can accommodate the SANG access road and car park without adversely affecting veteran trees. Condition 9 ensures the continued retention of high quality trees.

**Access and Movement Considerations:**

7. Following a query at the June Committee, the highways officer has reconfirmed that they are satisfied with the size of the car park and the access arrangements to the proposed SANG.

**Maintenance of the SANG**

8. The SANG would be a windfall privately operated open space, and at this stage no housing is dependent upon this proposed SANG. As such its maintenance and whether or not it is even open or not is purely at the discretion of the applicant. Thus, it is not reasonable or necessary to require maintenance payments for the SANG at this stage and requiring payments would fail the tests of the NPPG with regards planning obligations.
9. If at a future stage a housing application was approved in the vicinity that was dependent upon the SANG for SPA mitigation; a legal agreement could be considered on any potential housing approval which would secure the maintenance of the SANG in perpetuity. This cannot however be required at this stage for the reasons outlined above.

**RECOMMENDATION**

**That the Committee grant planning permission subject to the following conditions:**

***Time Frame:***

1. The Development hereby permitted shall be commenced before the expiration of three years from the date of this permission.

*Reason: In pursuance of s91 of the Town & Country Planning Act 1990 (as amended by S51 of the Planning & Compensation Act 2004)*

***Landscaping and SANG Details:***

2. Prior to the commencement of the development, full details of both hard and soft landscape proposals shall be submitted to and approved in writing by the local planning authority. These details shall include:

- Means of enclosure/boundary treatments
- Details of vehicle and pedestrian access, circulation areas & hard surfacing materials
- minor artefacts and structure (e.g. furniture, play equipment, dog waste bins refuse or other storage units, signs, lighting, external services, etc).
- Details of path surfacing materials
- Maintenance access points and routes
- Details of benches/seating
- Soft landscaping details shall include planting plan, specification (including cultivation and other operations associated with plant and grass establishment), schedules of plants, noting species, planting sizes and proposed numbers/densities where appropriate, and implementation timetable.
- Details of biodiversity enhancement areas (preferably including an area of meadow restoration)

All hard and soft landscape works shall be carried out in accordance with the approved details prior to the use of any part of the development or in accordance with a timetable approved in writing by the local planning authority. Any trees or plants that, within a period of five years after planting, are removed, die or become seriously damaged or defective, shall be replaced in the next planting season with others of species, size and number as originally approved and permanently retained.

*In the interests of amenity and to ensure the SANG is of sufficient quality to mitigate any future local development . Relevant policy: Core Strategy policy CP3 and Managing Development Delivery Local Plan policies CC03 and TB21 (and TB06 for garden development)*

3. No trees, shrubs or hedges within the site which are shown as being retained on the approved plans shall be felled, uprooted wilfully damaged or destroyed, cut back in any way or removed without previous written consent of the local planning authority; any trees, shrubs or hedges removed without consent or dying or being severely damaged or becoming seriously diseased within 10 years from the completion of the development hereby permitted shall be replaced with trees, shrubs or hedge plants of similar size and species unless the local planning authority gives written consent to any variation.

*Reason: To secure the protection throughout the time that development is being carried out, of trees, shrubs and hedges growing within the site, which are of amenity value to the area.*

*Relevant policies: Core Strategy policy CP3 and Managing Development Delivery Local Plan policies CC03 and TB21*

**Plan numbers:**

4. This permission is in respect of the following drawings: '6221-L-05 (Location Plan)' and '6221-L-03 rev C (SANG PROPOSALS)'. The development hereby permitted shall be carried out in accordance with the approved details unless otherwise agreed in writing by the Local Planning Authority

The development hereby permitted shall be carried out in accordance with the approved details unless otherwise agreed in writing by the Local Planning Authority.

*Reason: To ensure that the development is carried out in accordance with the application form and associated details hereby approved.*

### **Drainage Conditions**

5. The car parking area as well as the route through the SANG should be of permeable construction and development should not begin until details of both constructions have been submitted to and approved in writing by the LPA.

*Reason: This is to prevent increased flood risk from surface water run-off. Relevant policy: NPPF Section 10 (Meeting the Challenge of Climate Change, Flooding and Coastal Change), Core Strategy policy CP1 and Managing Development Delivery Local Plan policies CC09 and CC10*

### **Transport Conditions**

6. Prior to the commencement of the development hereby permitted details of a lockable height barrier shall be submitted to and approved in writing by the local planning authority. The lockable height barrier shall be implemented in accordance with such details as may be approved before first use of the SANG hereby permitted, and shall be permanently retained in the approved form for the lifetime of the development.

*Reason: In the interests of security, highway safety, to ensure proper usage of the site. Relevant policy: Core Strategy policies CP3 & CP6.*

7. No part of the SANG hereby permitted shall be occupied/used until vehicle parking and turning space has been provided in accordance with details to be submitted to and approved in writing by the local planning authority. The vehicle parking and turning space so-approved shall be retained in accordance with the approved details and the parking space remain available for the parking of vehicles at all times and the turning space shall not be used for any purpose other than vehicle turning.

*Reason: To provide adequate off-street vehicle parking and turning space and to allow vehicles to enter and leave the site in a forward gear in the interests of road safety and convenience and providing a functional, accessible and safe development and in the interests of amenity. Relevant policy: Core Strategy policies CP3 & CP6 and Managing Development Delivery Local Plan policy CC07.*

8. Prior to the commencement of the development hereby permitted details of secure and covered bicycle storage/ parking facilities for visitors to the SANG shall be submitted to and approved in writing by the local planning authority. The cycle storage/ parking shall be implemented in accordance with such details as may be approved before first use of the SANG hereby permitted, and shall be permanently retained in the approved form for the parking of bicycles and used for no other purpose.

*Reason: In order to ensure that secure weather-proof bicycle parking facilities are provided so as to encourage the use of sustainable modes of travel. Relevant policy: NPPF Section 4 (Sustainable Transport) and Core Strategy policies CP1, CP3 & CP6 and Managing Development Delivery Local Plan policy CC07.*

9. *Protection of trees*

- a) No development, other operation or works to trees shall commence on site until a revised and updated Tree Survey and Arboricultural Method Statement which provides for the retention and protection of trees (including those adjacent to Finchampstead Road), shrubs and hedges growing on or adjacent to the site in accordance with BS5837: 2012 has been submitted to and approved in writing by the local planning authority. No development or other operations shall take place except in complete accordance with the details as so-approved (hereinafter referred to as the Approved Scheme)
- b) No development (including any tree felling, tree pruning, demolition works, soil moving, temporary access construction and or widening or any other operation involving use of motorised vehicles or construction machinery) shall commence until the local planning authority has been provided (by way of a written notice) with a period of no less than 7 working days to inspect the implementation of the measures identified in the Approved Scheme on-site.
- c) No excavations for services, storage of materials or machinery, parking of vehicles, deposit or excavation of soil or rubble, lighting of fires or disposal of liquids shall take place within an area designated as being fenced off or otherwise protected in the Approved Scheme.
- d) The fencing or other works, which are part of the Approved Scheme, shall not be moved or removed, temporarily or otherwise, until all works including external works have been completed and all equipment, machinery and surplus materials removed from the site, unless the prior approval of the local planning authority has first been sought and obtained.

*To secure the protection throughout the time that the development is being carried out of trees shrubs or hedges growing within or adjacent to the site which are of amenity value to the area, and to allow for verification by the local planning authority that the necessary measures are in place before development and other works commence Relevant policy: Core Strategy policy CP3 and Managing Development Delivery Local Plan policies CC03 and TB21*

10. Prior to the commencement of the development a landscape management plan, including long term design objectives, management responsibilities, timescales and maintenance schedules for all landscape areas, other than privately owned, domestic gardens, shall be submitted to and approved in writing by the local planning authority. The landscape management plan shall be carried out as approved.

*Reason: In order to ensure that provision is made to allow satisfactory maintenance of the landscaping hereby approved. Relevant policy: Core Strategy policy CP3 and Managing Development Delivery Local Plan policies CC03 and TB21*

<b>CONTACT DETAILS</b>		
<b>Service</b>	<b>Telephone</b>	<b>Email</b>
Development Management and Regulatory Services	0118 974 6428 / 6429	development.control@wokingham.gov.uk

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<b>Development Management Ref No</b>	<b>No weeks on day of committee</b>	<b>Parish of</b>	<b>Ward</b>	<b>Listed by:</b>
170264	20/20	Wokingham	Wescott	Scheme of Delegation

**Applicant Location** Gladman Developments  
Land East Of Finchampstead Road, **Postcode** RG40 3HE  
Wokingham

**Proposal** Full application for the proposed change of use of land to Suitable Alternative natural greenspace (SANG) with associated landscape works and parking.

**Type** Full  
**PS Category** 6 – Major (All other Developments)  
**Officer** Ashley Smith

**FOR CONSIDERATION BY** Planning Committee on 14/06/17  
**REPORT PREPARED BY** Head of Development Management and Regulatory Services

#### SUMMARY

The application proposes the change of use of 11.23 hectares of undeveloped land on the eastern side of Finchampstead Road to a Suitable Alternative Natural Greenspace (SANG).

The proposal is considered acceptable in highway, character, drainage and amenity terms and is considered capable as acting as a SANG and providing an informal leisure facility. The proposal therefore accords with the policies contained within the Development Plan and national planning policy as such this application is recommended for approval.

The application is brought before the committee as it is a major development recommended for approval.

#### PLANNING STATUS

- Countryside
- Thames Basin Heath Special Protection Area (5km and 7km)
- Mineral Site Consultation Zone
- Aerodrome Safeguarding for wind turbines

#### RECOMMENDATION

**That planning permission be granted subject to the following conditions:**

**Time Frame:**

1. The Development hereby permitted shall be commenced before the expiration of

three years from the date of this permission.

*Reason: In pursuance of s91 of the Town & Country Planning Act 1990 (as amended by S51 of the Planning & Compensation Act 2004)*

**Landscaping and SANG Details:**

2. Prior to the commencement of the development, full details of both hard and soft landscape proposals shall be submitted to and approved in writing by the local planning authority. These details shall include:

- Means of enclosure/boundary treatments
- Details of vehicle and pedestrian access, circulation areas & hard surfacing materials
- minor artefacts and structure (e.g. furniture, play equipment, dog waste bins refuse or other storage units, signs, lighting, external services, etc).
- Details of path surfacing materials
- Maintenance access points and routes
- Details of benches/seating
- Soft landscaping details shall include planting plan, specification (including cultivation and other operations associated with plant and grass establishment), schedules of plants, noting species, planting sizes and proposed numbers/densities where appropriate, and implementation timetable.
- Details of biodiversity enhancement areas (preferably including an area of meadow restoration)

All hard and soft landscape works shall be carried out in accordance with the approved details prior to the occupation of any part of the development or in accordance with a timetable approved in writing by the local planning authority. Any trees or plants which, within a period of five years after planting, are removed, die or become seriously damaged or defective, shall be replaced in the next planting season with others of species, size and number as originally approved and permanently retained.

*In the interests of amenity and to ensure the SANG is of sufficient quality to mitigate any future local development . Relevant policy: Core Strategy policy CP3 and Managing Development Delivery Local Plan policies CC03 and TB21 (and TB06 for garden development)*

3. No trees, shrubs or hedges within the site which are shown as being retained on the approved plans shall be felled, uprooted wilfully damaged or destroyed, cut back in any way or removed without previous written consent of the local planning authority; any trees, shrubs or hedges removed without consent or dying or being severely damaged or becoming seriously diseased within 10 years from the completion of the development hereby permitted shall be replaced with trees, shrubs or hedge plants of similar size and species unless the local planning authority gives written consent to any variation.

*Reason: To secure the protection throughout the time that development is being carried out, of trees, shrubs and hedges growing within the site which are of amenity value to the area.*

*Relevant policies: Core Strategy policy CP3 and Managing Development Delivery Local Plan policies CC03 and TB21*

**Plan numbers:**

4. This permission is in respect of the following drawings:

Drawing No	Title
<i>Full schedule of all relevant plans to be included in Member's update.</i>	

The development hereby permitted shall be carried out in accordance with the approved details unless otherwise agreed in writing by the Local Planning Authority.

*Reason: To ensure that the development is carried out in accordance with the application form and associated details hereby approved.*

**Drainage Conditions**

5. The car parking area as well as the route through the SANG should be of permeable construction and development should not begin until details of both constructions have been submitted to and approved in writing by the LPA.

*Reason: This is to prevent increased flood risk from surface water run-off. Relevant policy: NPPF Section 10 (Meeting the Challenge of Climate Change, Flooding and Coastal Change), Core Strategy policy CP1 and Managing Development Delivery Local Plan policies CC09 and CC10*

**Transport Conditions**

6. Prior to the commencement of the development hereby permitted details of a lockable height barrier shall be submitted to and approved in writing by the local planning authority. The lockable height barrier shall be implemented in accordance with such details as may be approved before first use of the SANG hereby permitted, and shall be permanently retained in the approved form for the lifetime of the development.

*Reason: In the interests of security, highway safety, to ensure proper usage of the site. Relevant policy: Core Strategy policies CP3 & CP6.*

7. No part of the SANG hereby permitted shall be occupied/used until vehicle parking and turning space has been provided in accordance with details to be submitted to and approved in writing by the local planning authority. The vehicle parking and turning space so-approved shall be retained in accordance with the approved details and the parking space remain available for the parking of vehicles at all times and the turning space shall not be used for any purpose other than vehicle turning.

*Reason: To provide adequate off-street vehicle parking and turning space and to allow vehicles to enter and leave the site in a forward gear in the interests of road safety and convenience and providing a functional, accessible and safe development and in the interests of amenity. Relevant policy: Core Strategy policies CP3 & CP6 and Managing Development Delivery Local Plan policy CC07.*

8. Prior to the commencement of the development hereby permitted details of secure and covered bicycle storage/ parking facilities for visitors to the SANG shall be submitted to and approved in writing by the local planning authority. The cycle storage/ parking shall be implemented in accordance with such details as may be approved before first use of the SANG hereby permitted, and shall be permanently retained in the approved form for the parking of bicycles and used for no other

purpose.

*Reason: In order to ensure that secure weather-proof bicycle parking facilities are provided so as to encourage the use of sustainable modes of travel. Relevant policy: NPPF Section 4 (Sustainable Transport) and Core Strategy policies CP1, CP3 & CP6 and Managing Development Delivery Local Plan policy CC07.*

## **9. Protection of trees**

- a) No development, other operation or works to trees shall commence on site until a revised and updated Tree Survey and Arboricultural Method Statement which provides for the retention and protection of trees (including those adjacent to Finchampstead Road), shrubs and hedges growing on or adjacent to the site in accordance with BS5837: 2012 has been submitted to and approved in writing by the local planning authority. No development or other operations shall take place except in complete accordance with the details as so-approved (hereinafter referred to as the Approved Scheme)
- b) No development (including any tree felling, tree pruning, demolition works, soil moving, temporary access construction and or widening or any other operation involving use of motorised vehicles or construction machinery) shall commence until the local planning authority has been provided (by way of a written notice) with a period of no less than 7 working days to inspect the implementation of the measures identified in the Approved Scheme on-site.
- c) No excavations for services, storage of materials or machinery, parking of vehicles, deposit or excavation of soil or rubble, lighting of fires or disposal of liquids shall take place within an area designated as being fenced off or otherwise protected in the Approved Scheme.
- d) The fencing or other works which are part of the Approved Scheme shall not be moved or removed, temporarily or otherwise, until all works including external works have been completed and all equipment, machinery and surplus materials removed from the site, unless the prior approval of the local planning authority has first been sought and obtained.

*To secure the protection throughout the time that the development is being carried out of trees shrubs or hedges growing within or adjacent to the site which are of amenity value to the area, and to allow for verification by the local planning authority that the necessary measures are in place before development and other works commence Relevant policy: Core Strategy policy CP3 and Managing Development Delivery Local Plan policies CC03 and TB21*

10. Prior to the commencement of the development a landscape management plan, including long term design objectives, management responsibilities, timescales and maintenance schedules for all landscape areas, other than privately owned, domestic gardens, shall be submitted to and approved in writing by the local planning authority. The landscape management plan shall be carried out as approved.

*Reason: In order to ensure that provision is made to allow satisfactory maintenance of the landscaping hereby approved. Relevant policy: Core Strategy policy CP3 and Managing Development Delivery Local Plan policies CC03 and TB21*

## PLANNING HISTORY

No relevant planning history on this site.

An outline application has been received on the adjacent land for 250 houses this application has yet to be determined and has the reference **170261**.

## SUMMARY INFORMATION

### For Commercial

Site Area	11.23 Hectares
Existing use	Undeveloped rural land
Proposed use	Use Class D2 – Assembly and Leisure
Public Open Space proposed	11.23 Hectares
Existing parking spaces	Nil
Proposed parking spaces	10

## CONSULTATION RESPONSES

WBC Highways	No objection, subject to conditions
WBC Environmental Health	No objections or conditions recommended.
WBC Trees and Landscapes	No objection subject to conditions
WBC Drainage	No objection subject to conditions
Natural England	No objection, to the SANG as a standalone development
Wokingham Town Council	“Objection on the grounds that this application has been made to support an unsustainable development.”  (Officer note application must be considered on its own merits)
Ward Members	Cllr Whittle and Cllr McGhee-Sumner sent the following comments:  “The proposal outlined in the above planning application falls within Wescott Ward. As the Town and Borough Councillors representing the residents of Wescott Ward we are very supportive of their objections to this planning application.  The application is for a Suitable Alternative Natural Greenspace (SANG). However, the area is already an open field, water meadow, grassland and trees.  More important is that the proposed SANG only covers approximately one third of the total area currently open field, water meadow, grassland and trees.  Apart from being an area of green that breaks up the mature houses along the Finchampstead Road, this area also acts as an important “sponge” to absorb surface water.

	<p>The report from Lees Roxburgh Limited highlights that there is currently a high risk of flooding from surface water to the north of the SANG. The residents of Luckley Wood can testify to this.</p> <p>If the proposed development of 250 dwellings goes ahead the area of open field, water meadow, grassland and trees will be reduced by two thirds and the surface water from the “concrete jungle” of the development will flow into the surrounding areas. With an already high risk of flooding from surface water identified by Lees Roxburgh Limited the situation will be exacerbated from the water flow coming from the proposed development and increase the frequency of flooding as the much reduced area of grassland will not be able to cope.</p> <p>We are opposed to the substantial reduction in area of grasslands and are concerned that the SANG will not be able to accommodate the additional surface water flowing from the proposed adjoining development. Increased flooding risk will result to the detriment of existing residents.</p> <p>For all of the above reasons we believe that this application should be rejected.”</p>
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## REPRESENTATIONS

An outline application has been received on the adjacent land for 250 houses this application has yet to be determined and has the planning reference number **170261**.

410 Letters of objection/comment have been received. However the overwhelming majority of these letters covers both these sites. Objection in these letters primarily focus on the housing and concerns that the SANG might enable the housing (Officer note; this is not reasonable grounds to object to the standalone application):

Issues pertaining to the housing application are not summarised below, these will be considered in the assessment for that development.

The following Planning issues were raised about the SANG:

- Land should be left in its’ natural state. One of the few untainted areas around Wokingham. Land was part of an unsuccessful village green Inquiry.
- SANG is not necessary in isolation/Sufficient SANG in the area already.
- SANG would be out of character with countryside and would introduce unnecessary paraphernalia to the countryside
- Unsustainable location
- Poor Drainage at the site
- Access is unsuitable
- Traffic impact unacceptable
- SANG is located in the Countryside and will have and unacceptable landscape and

visual impact

- Will remove strategic gap between settlements
- SANG will affect outlook/residential amenity
- Impact on vegetation
- Impact on Ecology from the development of the site for SANG
- Potential impact on trees

## **APPLICANTS POINTS**

The proposal for the site is for a change of use of the land to provide a new area of publically accessible open green space to act as a SANG to the nearby Thames Basin Heaths Special Protection Area (SPA), to mitigate potential impacts which could arise from new residential development generating additional use of the SPA. The SPA is located approximately 4km to the south east of the site boundary at its closest point.

The proposals will provide significant biodiversity enhancements compared to the existing status of the site, both in terms of the habitats present and the opportunities that the proposal presents for species such as nesting and foraging birds, foraging bats, reptiles and mammals. The proposal will also facilitate biodiversity benefits associated with the Thames Basin Heaths SPA given that it will not only divert potential future use associated with proposed housing but it will also divert existing use of the SPA by existing residents close to the SANG site.

## **PLANNING POLICY**

### National policies

National Planning Policy Framework (NPPF)  
National Planning Policy Guidance (NPPG)

### Saved South East Plan Policy

NRM6: Thames Basin Heath Special Protection Area

### Wokingham Borough Core Strategy:

CP1: Sustainable Development  
CP2: Inclusive Communities  
CP3: General Development Principles  
CP6: Managing Travel Demand  
CP7: Biodiversity

### MDD DPD Local Plan:

CC02: Development Limits  
CC04: Sustainable Design and construction

CC06: Noise  
CC05: Renewable energy and decentralised energy networks  
CC07: Parking  
CC09: Flood Risk  
CC10: Sustainable Drainage  
TB08: Open Space, sport and recreational facilities standards  
TB21: Landscape Character  
TB23: Biodiversity and development

## **PLANNING ISSUES**

### **Preliminary Matter:**

1. An outline application has been received on the adjacent land for 250 houses. This application has yet to be determined and has the reference 170261. The housing application in particular has received a significant amount of objection. This is a separate planning application and must be determined on its own merits.

### **Principle of Development:**

2. The proposal is for use of the land for informal recreation. Policy CP11 states that proposals that contribute and/or promote recreation in, and enjoyment of, the countryside are generally acceptable in principle subject to the impact on the rural character of the area. The site is currently countryside and will remain so, the use of the land as a SANG will improve nature quality and promote public access to the site and as such is not considered harmful to the countryside.
3. The primary purpose of a SANG is to provide an alternative to the sites for informal recreation within the Special Protection Area and thus it would contribute towards conserving sites of international nature conservation importance (European Sites), as required by saved South East Plan Policy NRM6 and Core Strategy Policies CP7 & CP8.
4. The site is located in the countryside to the south of the major Development Location of Wokingham. To meet the requirements for a SANG public access would be provided through a network of footpaths alongside the management and provision of the open space and vegetation. Thus the proposal will improve the management of the countryside and biodiversity, protect and enhance woodland and provide opportunities for access to the countryside for informal recreational on the urban fringe, in compliance with saved South East and NRM6, and Core Strategy Policy CP8.
5. The use of the land for informal recreation and the associated ancillary development area acceptable in principle in the Countryside subject to the detail discussed below.

### **Impact on countryside & character:**

6. Core Strategy Policies CP1 and CP3, require new development to maintain or enhance the high quality of the environment. Policy CP11 requires development to maintain the quality of the environment in the countryside. Proposals should respect the landscape character of the area, retain and protect existing landscape features such as trees and woodland and provide for appropriate landscaping, which should generally consist of locally native species.

7. The site extends to 11.23 ha and is located to the south of the residential area of Luckley Wood to the south of Wokingham. It is accessed from Finchampstead Road which is located to the west of the site. The eastern boundary of the site is formed by the North Downs railway line.
8. The site currently comprises areas of grassland, woodland and scrub, and is situated to the north of Wokingham Family Golf Course. The site is served by an existing access track leading from Finchampstead Road to the west. The access track also serves the nearby golf course. As the proposal seeks to utilise the current access to the site no significant visible urbanisation will be apparent from public view points along the Finchampstead Road.
9. The proposed design of the SANG includes the retention and enhancement of habitats which are already present within the site, including rough grassland, wet grassland, a stream, woodland and scrub. New areas of native hedgerow and scrub planting are also proposed in the site to add further interest. A new car park for use by visitors is proposed to the west of the site, and would be accessed via the existing track leading from Finchampstead Road, further to the west. The car park would be well screened from public view points by mature vegetation. The SANG proposals also include the provision of some lengths of boardwalk over the stream running through the site and areas of wet ground to ensure the circular route remains passable, as well as provision of information boards, crossing points, way markers and bins.
10. The proposal will retain the rural verdant feel of the site; and the paraphernalia associated with the development is proportionate and to be expected to support the recreational activities at a SANG. Conditions 2 & 3 ensure landscape and SANG quality is maintained and that vegetation at the site is maintained. As a result there are no concerns with regards character and urbanisation of the countryside and the proposal is considered compliant with Core Strategy Policy CP11.

**Impact on Neighbours:**

11. Built development would be limited to the construction of the car park and footpath network, so the main impact of the proposal on occupants of nearby dwellings would be from increased activity at the site.
12. The access road serving the site already exists and serves Wokingham Family Golf Centre as well as a small number of dwellings. There would be a limited increase in movements to the site and the nearest residential property is approximately 130m from the proposed car park. Similarly the nearest properties to the SANG are at Luckley Wood and are located some 40m from the northern boundary of the site. No lighting has been proposed as part of the proposals. Given the separation distances involved and the nature of the use at the site it is considered that the proposal would not cause any harm to residential amenity.

**Ecology:**

13. The intention is that the proposed public open space would be used as a SANG. As such the proposal would assist in mitigating the impact on the Special Protection Area as required by Saved South East Plan Policies NRM6 together with Core Strategy policies CP7 and CP8.

14. Core Strategy Policy CP7 requires the impact on wildlife to be considered when assessing development proposals. The nature conservation value of sites should be taken into account when assessing proposals for development and, where appropriate, the creation and future management of new wildlife habitats and corridors should be secured. The proposal will provide new habitats and management of existing habitats, so would comply with these policies, this is further secured by condition 2.
15. The development would increase footfall to the site however this is mitigated by increase mitigation measures and improvements to the quality of habitats. Subject to final details of biodiversity enhancements being agreed as part of condition 2 the Borough Ecologist is satisfied that the proposal would not adversely affect biodiversity at the site and the proposal would bring long term benefit to biodiversity in the area. Natural England has raised no objection to the proposal as a standalone SANG.
16. Should any housing be approved at a future stage which is dependent on the capacity of this SANG then further details of management of the SANG and a mechanism for securing its upkeep would need to be secured via a legal agreement for that housing application. This is not however required for this standalone SANG application.

**Access and movement:**

17. The proposal seeks to utilise an existing access. The Highway Authority considers that the proposal will not cause any significant impact upon the local highway network and that total vehicle movements for the SANG would be relatively limited. The SANG is well located near to an existing Major development location and can be accessed by local residents on foot, this is considered to be a sustainable location for a use of this type.
18. The proposal would utilise an existing access of acceptable quality for the level of use and this is considered to be satisfactory by the Highway Authority who has no significant concerns over the very limit increased intensity of use of the access caused by the proposal.
19. The proposal includes a new 10 space car park specifically dedicated to use of the SANG. The detailed arrangements for the design and operation of the car park area and bicycle parking are secured by conditions 7 and 9.

**Trees and Landscape:**

20. The development utilises the existing access to the site and as such does not lead to the loss of trees at the entrance to the site. The same tree report was submitted with this application is the same as the report submitted with application 170261 (Which would have greater impact upon trees) and as such a revised report is required prior to the commencement of development, to clarify that all trees along Finchampstead Road are to be retained as part of the SANG development (condition 9).
21. Within the SANG the proposal will lead to maintenance of vegetation as well as additional planting to create habitats and enhancements. The Landscape Architect and Borough Ecologist have both reviewed the scheme and are satisfied with the proposals subject to the Landscape and Ecological conditions above.

**Flood risk and drainage:**

22. The Drainage Officer has reviewed the Lees Roxburgh Flood Risk Assessment 6086/R3, for the SANG, dated January 2017. It is noted that there will be no built development within the SANG area except for a car parking area and a walkway routes through the site.
23. The site lies within flood zone 1, where the probability of flooding is low. However it is recognised that historically parts of the land particularly adjacent to Luckley Wood have had some surface water drainage issues.
24. Having reviewed the information, the drainage officer is satisfied that the SANG development would not cause additional drainage issues or unacceptably increase flood risk in the vicinity, subject to condition 5 requiring the hard surfaces of the car park to be of a permeable nature.
25. Whilst it is noted that there have been pre-existing issues particularly at Luckley Wood with regards surface water drainage this proposal would not worsen the existing situation and as such a reason for refusal could not be substantiated on these grounds.

**Mineral extraction consultation zone:**

26. The proposed development is located in an area of the Borough where extraction of minerals could be possible. The Replacement Minerals Local Plan for Berkshire sets out the approach to planning for minerals which states that Local Planning Authorities will oppose development that would cause the sterilisation of minerals deposits, unless:-
- the mineral deposit is of no commercial interest
  - it can be demonstrated that there is an overriding need for development
  - there is likely to such strong environmental or other objection that extraction of minerals would be unlikely to be permitted
27. The proposal is for the development of a SANG. It is considered unlikely that the site would be used for commercial extraction given its constraints and surrounding residential uses, however in any case the land would remain undeveloped and as such this would not preclude mineral extraction from happening in the future.

**CONCLUSION**

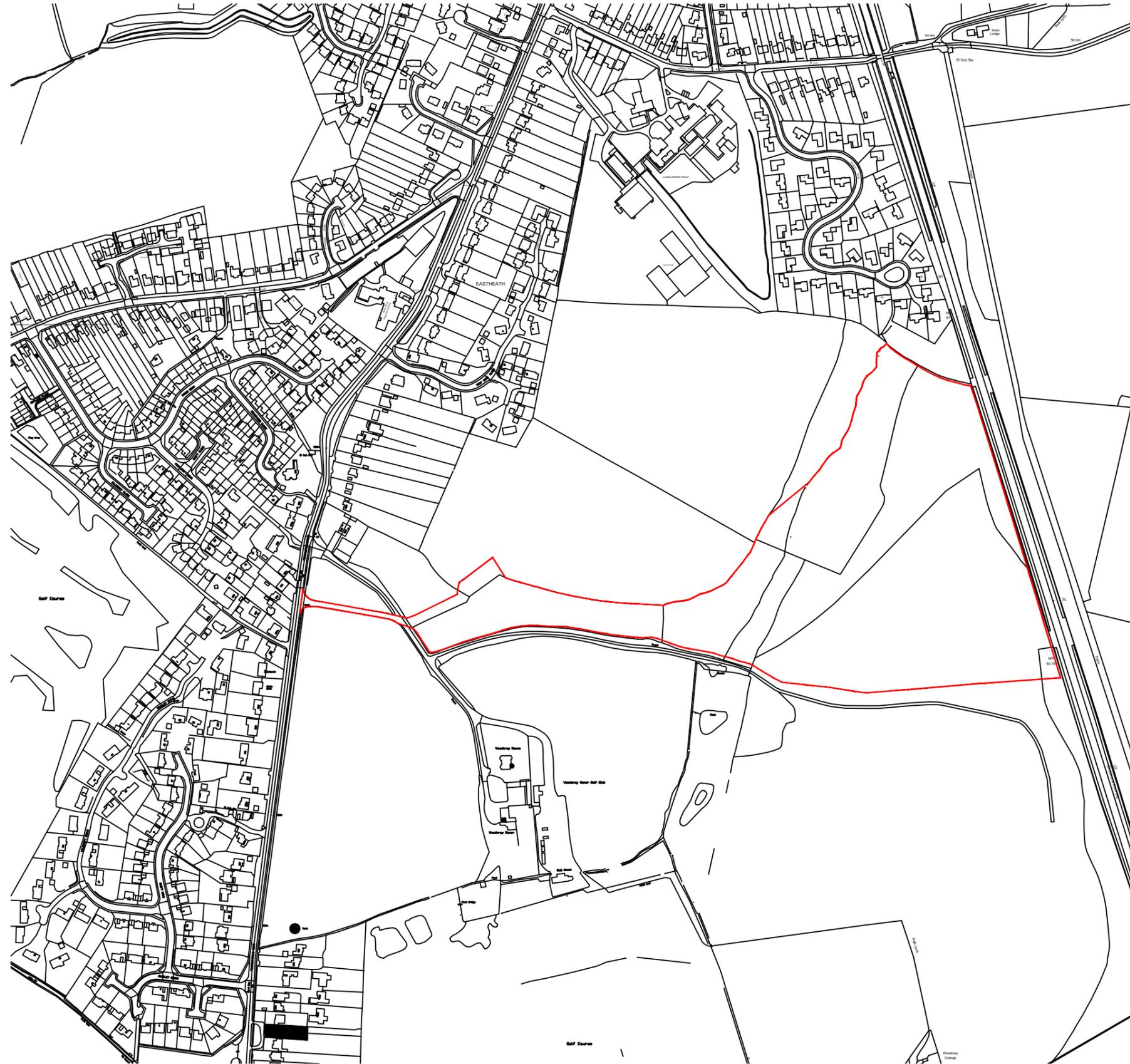
The application proposes the change of use of 11.23 hectares of undeveloped land on the eastern side of Finchampstead Road to a Suitable Alternative Natural Greenspace (SANG).

The proposal is considered acceptable in Highway, character, drainage and amenity terms and is considered capable as acting as a suitable SANG. The proposal therefore accords with the policies contained within the Development Plan and national planning policy as such this application is recommended for approval.

**CONTACT DETAILS**

Service	Telephone	Email
Development	0118 974 6428 / 6429	development.control@wokingham.gov.uk

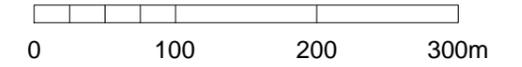
Management and Regulatory Services		
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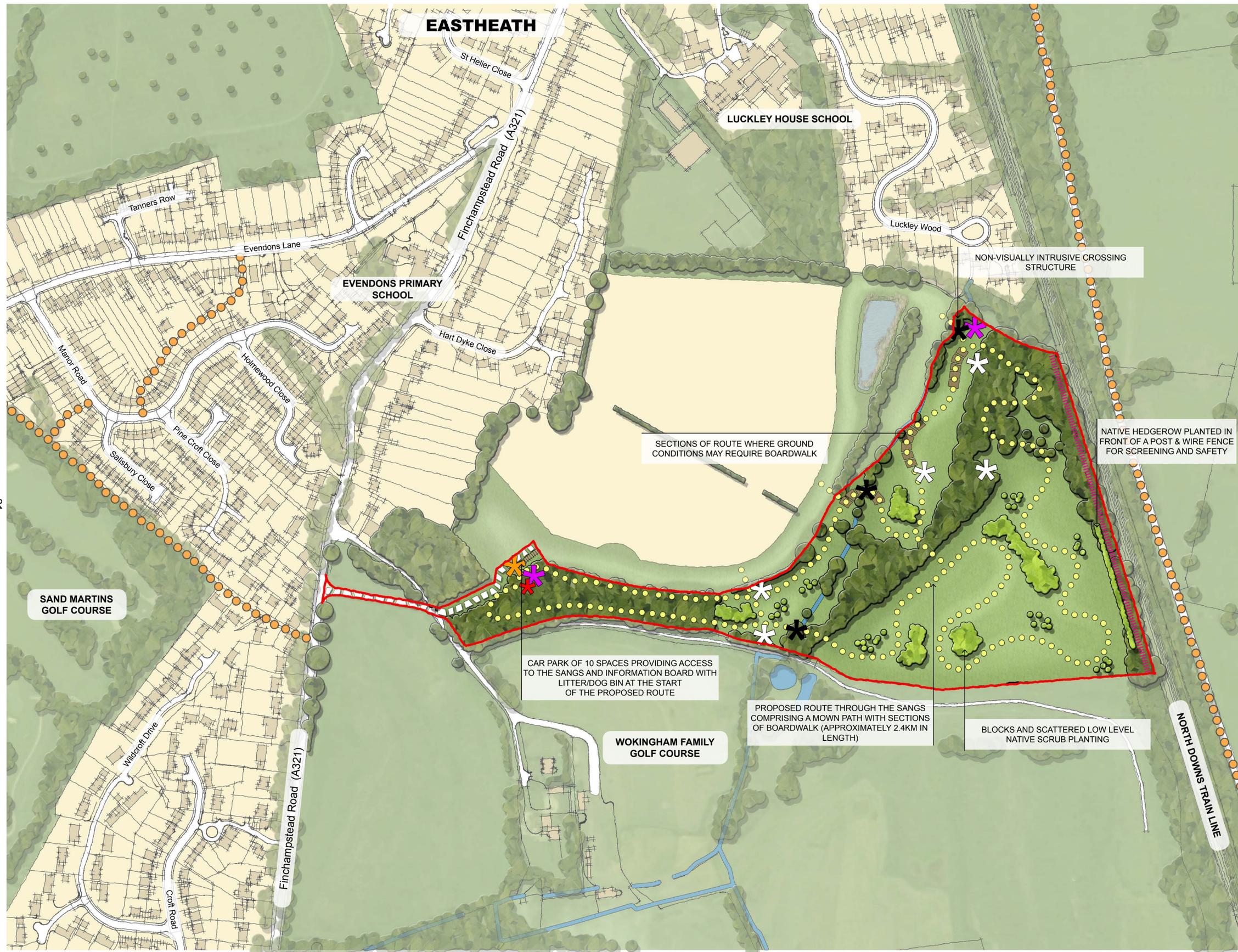
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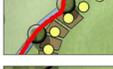
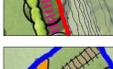
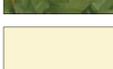
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Ordnance Survey base mapping - supplied by client.



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-  SANG BOUNDARY 11.23Ha
-  Existing Vegetation
-  Existing Public Rights of Way
-  Mown Circular Route (Total Length - 2.4km)
-  Sections of route where the ground conditions may require Boardwalk
-  Scattered Native Scrub Planting
-  Block Native Scrub Planting
-  Native Hedgerow / Tree Planting
-  Post and Wire Fencing
-  Car Park (10 Spaces) with access off existing lane serving Wokingham Family Golf Course
-  Visually Sensitive Waymarker
-  Crossing Structure Over Stream
-  Information Board
-  Dog / General Waste Bin
-  Proposed Development Area Adjacent to SANG

31



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**From:** Mike Dunstan  
**To:** [Planning Enquiries](#)  
**Cc:** [Ashley Smith](#)  
**Subject:** Comments on 170261 and 170264: Land to east of Finchampstead Road, Wokingham  
**Date:** 21 March 2017 16:44:38

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Wokingham Town Council has considered these applications and makes comment as follows.

**170261 Land to east of Finchampstead Road**

**Outline application for the proposed erection of up to 250 dwellings (including 40% affordable housing), landscaping, public open space and equipped play areas, surface water flood mitigation and attenuation, vehicular access from Finchampstead Road and associated ancillary works (Access to be considered).**

**Planning Officer: Ashley Smith**

Objection on the grounds that the proposed site is outside the development locations defined by Policy CP9 of the adopted Core Strategy and is not a sustainable location for the following reasons.

1. Finchampstead Road is already over capacity and this will get worse on completion of the Southern Distributor Road and Arborfield SDL. The Traffic Assessment is flawed because the survey was carried out on only one day in 2016 and the analysis contains the erroneous statement that spare capacity exists if the RFC value is less than 1.0. Typically a value of 0.85 is seen as the practical capacity, with results higher than this more likely to experience queuing or delay. The proposal therefore does not demonstrate that there is either sufficient infrastructure capacity or that this can be readily provided, contrary to Policy CP4 of the Adopted Core Strategy.
2. The existing bus service is inadequate for a development of this size and the proposal does not therefore comply with Policy CP6 of the adopted Core Strategy.
3. The loss of three high value TPO trees will harm the character of the Green Route in contravention of Policy CC03 of the adopted MDD.
4. The scale of the proposed development would be detrimental to the amenities of adjoining residents and their quality of life in contravention of Policy CP3 of the adopted Core Strategy.
5. The Flood Risk Assessment does not take account of the high water table and recent flooding events experienced by properties in Finchampstead Road and Luckley Wood and hence does not comply with Policy CP1 of the adopted Core Strategy.
6. Appeal reference APP/X0360/A/04/1150159 in 2005 against refusal for a club house and 31 dwellings on the adjacent Woodcray Manor golf course was dismissed on the grounds of volume of traffic on Finchampstead Road, highway safety of the access and sustainability.

**170264 Land to east of Finchampstead Road**

**Full application for the proposed change of use of land to Suitable Alternative natural greenspace (SANG) with associated landscape works and parking.**

**Planning Officer: Ashley Smith**

Objection on the grounds that this application has been made to support an unsustainable development.

Regards

Mike Dunstan  
Planning & Transportation Officer

Wokingham Town Council  
Town Hall  
Market Place  
Wokingham  
RG40 1AS

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2013-16 Silver Gilt Winner in Thames and Chilterns in Bloom

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# Agenda Item 17.

<b>Development Management Ref No</b>	<b>No weeks on day of committee</b>	<b>Parish</b>	<b>Ward</b>	<b>Listed by:</b>
170010	Ext	Shinfield	Shinfield South	N/A Major Development

**Applicant** Taylor Wimpey and David Wilson Homes

**Location** Land North of Croft Road, West of Hyde End Lane, North & South of Ryeish Lane, East of Clares Green Road Spencers Wood **Postcode** RG7 1DR

**Proposal** Reserved Matters application pursuant to Outline Planning Consent O/2013/0346 for 363 dwellings including internal access roads, garages, parking spaces, SUDS attenuation, open space, play areas and associated landscaping.

**Type** Major

**PS Category** 7

**Officer** Christopher Howard

**FOR CONSIDERATION BY** Planning Committee on 12<sup>th</sup> July  
**REPORT PREPARED BY** Delivery Programme Director

## **SUMMARY**

This reserved matters application follows several other planning consents for the South of the M4 SDL and the significant majority of the housing allocation and associated infrastructure required by the Core Strategy within the SDL has now been approved.

Specifically relevant to this site is the outline planning permission was approved for 900 residential units, a primary school, public open space and suitable alternative natural greenspace at Spencers Wood and Three Mile Cross under planning permission O/2013/0346. In addition, outline consent has been granted on appeal for 1,200 residential units and 150 extra care units at Shinfield West together with a primary school, local centre, public open space sports pavilion, suitable alternative natural greenspace (SANG) and access and landscaping. Both applications make significant contributions and interventions towards the delivery of the required infrastructure within the SDL which includes sports pitches, open space, education facilities and a new public transport service.

The outline application for this site also secured three areas of SANG – 5 Acre Field, The Ridge and Mays Farm and the latter is fully open and available for public use. In addition work for the Eastern Relief Road is well advanced ahead of the housing triggers imposed at the Shinfield West appeal. This has been achieved through forward funding (in the form of a loan) by the Homes and Communities Agency (HCA). There is further planning history for the SDL provided within the report.

The overarching vision of the SDL is to provide a co-ordinated approach to the delivery of infrastructure and services ensuring that developments are of a high quality and are sustainable. This includes the provision of schools, community facilities, good quality open space and appropriate local transport and links.

The principle of development in this location has been established through its allocation

by policy CP19 of the Core Strategy and through the Spatial Framework Plan within the South of the M4 Supplementary Planning Document together with the outline planning permission. In addition, the site is located within the development limits as identified in Policy 1 of Shinfield Parish Neighbourhood Plan

The application is before the Planning Committee as it relates to major development that is recommended for approval. It is considered that the development would be a sustainable urban extension to the existing settlement whereby residents would have good access to services and facilities. The development would not have a significant detrimental impact on the character of the area or on existing residents.

The proposal would provide for an adequate level of amenity for the future occupants of the dwellings together with an acceptable impact on ecology, traffic, highway safety and flood risk in addition to contributing to the delivery of infrastructure provided jointly by the wider SDL such as the Eastern Relief Road. In addition it would also assist in the delivery of housing numbers and assist the council in maintaining a 5 year housing supply.

In design terms, the proposal reflects other development parcels and meets all the standards and in particular internal space and parking. The proposals are considered to be acceptable, represents good design and therefore it is recommended that reserved matters are approved subject to conditions outlined below.

#### **PLANNING STATUS**

- Strategic Development Location (SDL) as identified on the Core Strategy
- South of the M4 Strategic Development Location SPD
- Infrastructure Delivery and Contributions SPD
- Within 5km of the Special Protection Area (SPA)
- Mineral consultation zone
- Nuclear Consultation Zone (HSE to be consulted on schemes of 200 dwellings or more)
- Grade II listed Building - Nullis Barn

#### **RECOMMENDATION**

APPROVE RESERVED MATTERS subject to the following conditions:

- 1) Nothing herein contained shall be deemed to affect or vary the conditions imposed by planning permission O/2013/0346 dated 22/07/14 which conditions shall remain in full force and effect save in so far as they are expressly affected or varied by this permission.
- 2) The development hereby permitted shall be carried out in accordance with the following approved plans:

Plan numbers being finalised to be included on members update

*Reason: For the avoidance of doubt and to ensure that the development is carried out in accordance with the application form and associated details hereby approved.*

## Highways

- 3) Prior to the commencement of development, details of the measures to restrict the vehicular access at the centre of the site (between road 3 and road 20 on plans A080-RM-41 and A080-RM-44 respectively shall be submitted and agreed in writing by the Local Planning Authority. This shall provide details to show that no vehicles other than emergency vehicles/bicycles/vehicles associated with the maintenance of the site for management of open space shall be permitted to access the site to the east and west of the site. The measures shall be implemented in accordance with the approved details prior to first occupation of the development and permanently so retained unless otherwise agreed in writing by the local planning authority.  
*Reason: In the interest of highway safety and condition 40 of the outline planning permission restricts access to 100 dwellings from Clairs Green Road. Relevant policy Wokingham Borough Core Strategy Policy CP6 and principles identified in Appendix 10 of the Spencers Wood and Three Mile Cross Environmental Statement pursuant to planning approval O/2013/0346*
- 4) No building shall be occupied until the vehicular access that serves the dwelling has been surfaced with bonded material across the entire width of the access.  
*Reason: To avoid spillage of loose material onto the highway, in the interests of road safety. Relevant policy: Core Strategy policy CP6.*
- 5) Notwithstanding the approved layout plan number CB\_81\_036\_01 Rev E, prior to commencement of development of plot 730 further details of the detailed setting out of this plot shall first be submitted for approval in writing by the local planning authority. This plot shall be built in accordance with the approved details unless otherwise agreed in writing by the local planning authority.  
*Reason: To ensure that roads and footpaths are constructed to a standard that would be suitable for adoption as publicly maintainable highway, in the interests of providing a functional, accessible and safe development. Relevant policy: Core Strategy policies CP3 & CP6.*
- 6) Notwithstanding the approved plans, prior to commencement of development of the apartment plots 571 to 585 revised details omitting the visitor parking bays and depicting the footway / cycleway running contiguous with the carriageway edge of Road 1 shall first be submitted for approval in writing by the local planning authority. Development shall be carried out in accordance with the approved details unless otherwise agreed in writing by the local planning authority.  
*Reason: To ensure that roads and footpaths are constructed to a standard that would be suitable for adoption as publicly maintainable highway, in the interests of providing a functional, accessible and safe development. Relevant policy: Core Strategy policies CP3 & CP6.*
- 7) Notwithstanding the approved plans, prior to commencement of development of apartment plots 530-542, further details of the cycle store located to the north-east of the apartment block shall first be submitted for approval in writing by the local planning authority. Development shall be carried out in accordance with the approved details unless otherwise agreed in writing by the local planning authority.  
*Reason: In order to ensure that secure weather-proof bicycle parking facilities are provided so as to encourage the use of sustainable modes of travel. Relevant policy: NPPF Section 4 (Sustainable Transport) and Core Strategy policies CP1,*

*CP3 & CP6 and Managing Development Delivery Local Plan policy CC07.*

- 8) Notwithstanding the approved plans, prior to first occupation of any plots of the development full details of the measures to prevent through traffic along the street between plots 754 and 772 and to dissuade through traffic along the street adjacent to plot 731 shall first be submitted to and approved in writing by the local planning authority. Development shall be carried out in accordance with the approved details unless otherwise agreed in writing by the local planning authority.  
*Reason: To ensure that roads and footpaths are constructed to a standard that would be suitable for adoption as publicly maintainable highway, in the interests of providing a functional, accessible and safe development. Relevant policy: Core Strategy policies CP3 & CP6.*

### **Landscaping**

- 9) Prior to the commencement of development, details of fencing for the rear / side boundaries of the existing dwellings that back/side onto the application site along Clairs Green Road, Croft Road and Ryeish Lane shall first be submitted for approval to the Local Planning Authority. The fencing shall be installed in accordance with the approved details prior to the formation of any foundations earlier date to be agreed with the Local Planning Authority and permanently so retained.  
*Reason: In the interests of visual and residential amenity and to ensure the proper planning of the site. Relevant policy: Core Strategy policies CP1 and CP3*

### **Permitted Development**

- 10) Those windows shown on the approved drawings as having obscured glazing in part or full shall be so-fitted and shall be permanently so-retained, and shall remain fixed closed at all times  
*Reason: To safeguard the residential amenities of neighbouring properties.  
Relevant policy: Core Strategy policy CP3*
- 11) Notwithstanding the provisions of the Town and Country Planning, (General Permitted Development) (England) Order 2015 (or any Order revoking and re-enacting that Order with or without modification), no additional windows or similar openings shall be constructed in the first floor level or above in any elevation of the buildings hereby permitted except for any which may be shown on the approved drawing(s).  
*Reason: To safeguard the residential amenities of neighbouring properties.  
Relevant policy: Core Strategy policy CP3.*

### **Informatives**

1. The applicant is advised that the Council seeks that employers or developers within the borough commit to using local labour / contractors where possible. This should include:
  - Advertisement of jobs within local recruitment agencies / job centres;
  - Recruitment and training of residents from the local area;
  - Seek tender of local suppliers or contractors for work.
2. The applicant is advised that the reserved matters planning approval should be read in conjunction with the S106 for the outline planning application for planning

permission O/2013/0346.

3. The applicant is informed that parking may need to be restricted along the main routes and on turning heads.
4. All the new dwellings should be built to 'Secured by Design' part 2 accreditation in the interests of the safety, crime prevention and amenity of future occupiers of the development. National sustained research proves that Secured by Design housing developments suffer at least 50% less burglary, 25% less vehicle crime and 25% less criminal damage.
5. **Work on Highway**  
The Corporate Head of Environment at the Council Offices, Shute End, Wokingham should be contacted for the approval of the access construction details before any work is carried out within the highway. This planning permission does NOT authorise the construction of such an access.
6. **Mud on Road**  
Adequate precautions shall be taken during the construction period to prevent the deposit of mud and similar debris on adjacent highways. For further information contact Corporate Head of Environment on tel: 0118 974 6302.
7. **Highway Adoption**  
If it is the developer's intention to request the Council, as local highway authority, to adopt the proposed access roads etc. as highway maintainable at public expense, then full engineering details must be agreed with the Corporate Head of Environment at the Council Offices, Shute End, Wokingham. The developer is strongly advised not to commence development until such details have been approved in writing and a legal agreement is made with the Council under S38 of the Highways Act 1980.
8. **Highway Management**  
Any works/events carried out either by, or at the behest of, the developer, whether they are located on, or affecting a prospectively maintainable highway, as defined under Section 87 of the New Roads and Street Works Act 1991, or on or affecting the public highway, shall be coordinated under the requirements of the New Roads and Street Works Act 1991 and the Traffic management Act 2004 and licensed accordingly in order to secure the expeditious movement of traffic by minimising disruption to users of the highway network in Wokingham.
9. **Utilities**  
Any such works or events commissioned by the developer and particularly those involving the connection of any utility to the site, shall be co-ordinated by them in liaison with Wokingham Borough Council's Street Works Team, (telephone 01189 746302). This must take place at least three month in advance of the works and particularly to ensure that statutory undertaker connections/supplies to the site are coordinated to take place wherever possible at the same time.
10. **Noise**  
The attention of the applicant is drawn to the requirements of Section 60 of the Control of Pollution Act 1974 in respect of the minimisation of noise on construction and demolition sites. Application, under Section 61 of the Act, for prior consent to

the works, can be made to the Environmental Health and Licensing Manager.

11. Employment and Skills plan should be provided in accordance with the Council's guidance. The applicant is advised to contact Angela Torr, Economic Sustainability Team, WBC.
12. Thames Water will aim to provide customers with a minimum pressure of 10m head (approx 1 bar) and a flow rate of 9 litres/minute at the point where it leaves Thames Waters pipes. The developer should take account of this minimum pressure in the design of the proposed development.

**Planning history - site / relating to overarching outline planning permission (O/2013/0346)**

O/2013/0346	Hybrid application with part in outline in respect of up to 900 dwellings up to a three form entry primary school public open space access and landscape (means of access into the site to be considered) and part in full in respect to Suitable Alternative Natural Greenspace (SANG) following demolition of existing dwelling and ancillary buildings including greenhouses sheds stables and garage (214a Hyde End Road). - Approved 22/07/2014
F/2013/0347 (outside application area but serves as access for part of development)	Full planning permission for the erection of 276 dwellings with associated access landscaping and open space following demolition of existing dwelling and ancillary buildings (214a Hyde End Road). - Approved 24/12/2013
162829	Application for Reserved matters application for 175 dwellings including internal access roads, garages, parking places, open spaces, allotments, Multi-Use Games Area (MUGA) and Locally Equipped Area of Play. Approved 10/03/17
163457	Reserved Matters application pursuant to Outline Planning Consent O/2013/0346 for the primary infrastructure phase including Spine Road, Suds Ponds, Foul and Surface Water Drainage and Associated Landscaping Approved 23/5/17

**Planning History South of M4 SDL**

F/2005/4827	Proposed construction of an Eastern relief road for Shinfield including a new bridge over the M4 motorway and under Cutbush Lane. Appeal withdrawn 31/10/2006
O/2007/2268	Outline application for the proposed residential development (C3 Use) at a minimum of 30 dph plus creation of new vehicular accesses footpath links cycleways and public open space. Approved 25/02/2008

O/2009/1027	Outline application for phase 1 development of Science & Innovation Park (Access to be considered) plus full application for the construction of access road foot and cycle ways M4 overbridge and associated works including landscaping and engineering works plus erection of boundary wall and fence adjoining Shinfield Road/Access Road. Part demolition and reformation of facade of Stable Buildings at Lane End Farm and demolition of existing farm buildings. – Approved 27/10/10
F/2010/1428	Full application for the construction of an Eastern Relief Road (ERR) to Shinfield including the construction of road foot and cycle-ways an M4 over-bridge. Re-grading of embankments landscaping utilities creation of flood compensation areas and associated works including engineering and other operations. Erection of replacement boundary wall and fence adjoining Shinfield Road/ ERR part demolition of existing farm buildings at Lane End Farm and demolition/deconstruction of two poly tunnels south of Cutbush Lane. – Appeal Approved 08/11/2012
F/2010/1434	Application for change of use of land from agricultural to Suitable Alternative Natural Greenspace (SANG) (Sui Generis use) and associated development to include Pedestrian and Vehicular access car park Footpaths and Landscaping. – Appeal Approved 08/11/2012
O/2010/1432	Outline application for a residential development of up to 1 200 dwellings a further 150 units of specialist housing (including sheltered housing) for elderly persons a local centre to include a foodstore (2 500 sqm) and other retail and office uses a community building proposed extension of existing primary schools erection of a new primary school public open space sports pavilion suitable alternative natural greenspace (SANG) and access and landscaping. – Appeal Approved 08/11/2012
O/2013/0565	Outline application for the erection of 100 dwellings with associated landscaping and open space means of access to be considered. – Approved 10/04/2014
O/2013/0101	Outline planning permission for a residential development comprising up to 126 dwellings, public open space, children's play areas, accesses to Cutbush Lane and the Shinfield Eastern Relief Road, Landscaping, Ecological buffer zone, ground modelling, sustainable drainage systems and associated infrastructure. – Approved 22/07/2014
VAR/2014/0624	Application to remove condition 48 of planning consent O/2010/1432 (the development shall be implemented in accordance with the approved Residential Travel Plan) – <i>Officer note: this has been removed, however a deed of variation has been agreed to the S106 agreement to secure contributions towards the councils own travel plan – My Journey.</i> Approved 22/10/14

F/2014/2323	Full permission erection of 9 dwellings north of Croft Road – Approved 23/03/15
NMT/2014/1837	Application for a non-material amendment to planning consent O/2010/1432 to allow for changes to condition 15 relating to Affordable Housing Provision – <i>Officer note: this application secured 16.6% on site affordable housing provision and a commuted sum for off-site affordable housing which has been secured through a deed of variation to the legal agreement.</i> Approved 23/10/14
RM/2015/1375	Reserved Matters application pursuant to Outline Planning consent VAR/2014/0624 for the erection of 69 dwellings including access roads garages parking spaces open space and landscape treatment of Phase 1a Shinfield West (access within site appearance landscaping layout and scale).
RM/2015/0630	Reserved Matters application pursuant to Outline Planning Consent O/2009/1027 for the development of phase 1A of proposed Thames Valley Science Park comprising the construction of a gateway building and all associated landscaping and ancillary works plus temporary car parking arrangements – Appearance, Landscaping, Layout and Scale to be considered.
160183	Application for Reserved Matters submission in respect of the primary infrastructure phase including access roads, suds ponds, foul and surface water drainage and associated landscaping. Approved 15/04/16
161189	Reserved Matters application pursuant to Outline planning permission VAR/2014/0624 (Pursuant to O/2010/1432) for the erection of 517 dwellings including access roads, garages, parking spaces, open space and landscape treatment of Phase 1, Shinfield West. (Access within the site, appearance, landscaping, layout and scale to be considered). Approved 21/07/2016
170035	Reserved Matters application for a two form entry (2FE) primary school, including all hard & soft play areas, school car parking, attenuation pond (habitat area), landscaping and associated drop off car Parking. Approved 11/05/17
170239	Application for approval of reserved matters following outline approval (O/2013/0346) for a sustainable transport link to serve public transport (buses), emergency vehicles and pedestrian and cycle links. Decision pending

<b>SUMMARY INFORMATION</b>	
<b>For Residential</b>	
Site Area	14.16
Site Area residential	9.4
Site Area Green infrastructure	4.7
Existing units	0

Proposed units	363
Proposed density - dwellings/hectare	31
Number of affordable units proposed	31
Previous land use	Agricultural / pasture
Proposed parking spaces	963

### CONSULTATION RESPONSES

WBC Archaeological officer	No objection
Natural England	No objection
WBC Environmental Health	No objection
Network Rail	No comments
Thames Water	No objection
Highways England	No objection
English Heritage	No comment
WBC Planning Policy	No comment
WBC Waste services	Standing advice in respect to refuse collection / bin collection points
WBC Highways/drainage	No objection, request conditions 3 - 8
WBC Landscape and trees	No objection, request condition 9
WBC Ecology	No objection
Shinfield Parish Council	<p>Concerns over the width of the spine road for the bus route and parking by school – <i>Officer Note: the spine road has been designed to be 6.1m wide for the entire length of the bus route. Visitor parking has been located in close proximity to the school and a traffic management plan will assist with on street parking – see report below paragraph 73</i></p> <p>Concerns over parking on the spine road – <i>Officer note: Parking management is controlled by condition 39 of the outline planning consent. There is visitor parking located on the spine road and roads adjacent to the spine road. Further parking has been provided for the apartments and the location of these is considered an acceptable approach to the overall design of the development with these located centrally to emphasise the key route and decrease the impact of the development on the edge of the settlement</i></p> <p>Concern over tandem parking – <i>Officer note: The development is in accordance with WBC Highway Standards and through negotiation officers have secured additional parking for the development - see report below paragraphs 76-79</i></p> <p>Request crossing points between the school and the play area – <i>Officer note: The plans have been revised to incorporate a further three crossing points in this location.</i></p> <p>Request garages to be 3x7m – <i>All garages comply with the 3 x 7m threshold as required by Highways</i></p> <p>Request suitable unallocated parking – <i>Officer note see report below paragraphs 76-79</i></p> <p>Request design variety – <i>Officer note: see report below paragraphs 27-35</i></p>

Berks Bucks and Oxford Wildlife Trust	No comments
WBC Conservation Officer	No comments
Environment Agency	No comments
WBC Affordable Housing	No objection
WBC Public Rights of Way	No objection
WBC Flood Risk Officer	No objection
Sport England	No comments

## REPRESENTATIONS

6 letters of objection –issues summarised below

- Impact on wildlife/ecology/trees – *Office note: The principle of development has been established for this site under the outline planning application and mitigation is proposed. The application has been assessed by the Biodiversity Officer and Landscape Officer and they have supported the application. See report below paragraphs 38-39 & 93-94*
- Loss of agricultural land / countryside /overdevelopment of the village – *Officer note: The principle of development on this site has been established through the outline application and allocation of the site through the south of the M4 SPD.*
- Impact on highways / highway safety – *Officer note: The principle of development has been established for this site under the outline planning application. This included a full Transport Assessment which assessed the impact on the highway. See report below paragraphs 70-82*
- Loss of views – *Officer note: Not a material planning consideration and cannot be used for a reason for refusal*
- Devaluation of property – *Officer note: not a material planning consideration*
- Flood risk/drainage – *Officer note: Drainage plans and a drainage statement have been considered. This has been assessed by the Flood Risk Officer and they have raised no objections to the proposed scheme. The flood risk assessment takes into account the impact of off-site drainage and seeks to ensure that these are lower than existing greenfield rates.*
- Loss of Light / Privacy – *Officer note: Fully assessed in paragraphs 42-59 below*
- Noise / Disruption / Construction Access – *These matters can be controlled by condition 24 of the outline planning consent.*
- Height of houses – *Officer note: the height of these have generally been established at the outline planning application stage. See report below paragraph33*
- Impact on Hyde End Lane and Ryeish Lane – *Officer note: a TRO application has been received to secure the appropriate measures to deliver the access strategy for these routes. Parts of these will be closed off to ensure adequate access. Access to all existing residential dwellings would be maintained and a turning head provided for refuse vehicles.*
- Fencing around existing residential dwellings – *Officer note: condition 9 controls*
- Access from Grovelands Road/Clairs Green Road should be limited to 70 dwellings and not 100 – *Officer note, condition 40 of the outline application secured access for 100 units and the applicant could still apply to deliver all of these and be within the parameters of access established at the outline stage. The current proposal falls below this threshold and is therefore acceptable.*
- Access to the development site at Clairs Green Road – *Officer note: this was*

*established at the outline stage. Details of the junction will be approved under the S278 (Highways Act) process.*

- *Impact on listed building- Officer note: The conservation officer has assessed the application and raised no objection to the proposed development. The S106 pursuant to the outline planning permission secures measures to preserve Nullis Barn. See report below paragraph 95*
- *Maintenance of Gas Main – Officer note: Southern Gas Networks are responsible for the maintenance of the Gas Main*

## **PLANNING POLICY**

### National Policy

National Planning Policy Framework 2012

Technical Guidance to the National Planning Policy Framework 2012

National Planning Policy Guidance 2014

### South East Plan 2009

Saved policy NRM6 - Thames Basin Heaths Special Protection Area

### Wokingham Borough Core Strategy policies:

CP1 - Sustainable Development

CP2 - Inclusive Communities

CP3 - General Development Principles

CP4 - Infrastructure Requirements

CP5 - Housing Mix, Density and Affordability

CP6 - Managing Travel Demand

CP7 - Biodiversity

CP8 - Thames Basin Heaths Special Protection Area

CP9 - Scale and Location of Development Proposals

CP10 - Improvements to the Strategic Transport Network

CP11 - Proposals outside Development Limits (including countryside)

CP17 - Housing delivery

CP19 – South of the M4 Strategic Development Location

Appendix 7 – Additional Guidance for the Development of Strategic Development Locations

### Managing Development Delivery Local Plan (MDD LP) policies

CC01 Presumption in Favour of Sustainable Development

CC02 Development Limits

CC03 Green Infrastructure, Trees and Landscaping

CC04 Sustainable Design and Construction

CC05 Renewable energy and decentralised energy networks

CC06 Noise

CC07 Parking

CC08 Safeguarding alignments of the Strategic Transport Network & Road Infrastructure

CC09 Development and Flood Risk (from all sources)

CC10 Sustainable Drainage

### Residential Uses

- TB05 Housing Mix  
TB07: Internal Space Standards  
TB09 Residential accommodation for vulnerable groups

### Landscape and Nature Conservation

- TB21: Landscape Character  
TB23: Biodiversity and Development

SAL05: Delivery of avoidance measures for Thames Basin Heaths Special Protection Area

### Shinfield Parish Neighbourhood Development Plan:

- Policy 1: Location of Development  
Policy 2: General Design Principles  
Policy 3: Sustainable Development  
Policy 4: Accessibility and Highway Safety  
Policy 5: Parking  
Policy 6: Trees, Hedgerows and Woodlands  
Policy 7: Biodiversity  
Policy 8: Flooding  
Policy 9: Community Assets  
Policy 10: Community and Sports Facilities  
Policy 12: Broadband Provision

### Supplementary Planning Documents

Wokingham Borough Design Supplementary Planning Document (18 February 2010)

Planning Advice Note, Infrastructure Impact Mitigation, Contributions for New Development (Revised 28th April 2010)

Sustainable Design and Construction Supplementary Planning Document (28 May 2010)

South of the M4 Strategic Development Location Supplementary Planning Document (October 2011)

Infrastructure Delivery and Contributions Supplementary Planning Document (October 2011)

Wokingham Borough Affordable Housing Supplementary Planning Document (2 June 2011)

A Vision for Our Villages: Ryeish Green, Spencers Wood, Three Mile Cross Character Statement

## **PLANNING ISSUES**

### *Principle of development and infrastructure delivery*

1. The application site forms part of a larger area designated under the Wokingham Borough Core Strategy as the South of the M4 Strategic Development Location (SDL).
2. Wokingham Borough Core Strategy policy CP17 establishes a requirement to provide at least 13,487 new dwellings with associated development and infrastructure in the period 2006-2026. The majority of this new residential

development will be in four SDLs, of which South of the M4 SDL is one of these. It is anticipated that a phased development incorporating approximately 2,500 dwellings and associated infrastructure will be delivered on 77 hectares of land within the South of the M4 SDL (Appendix A7.14).

3. Core Strategy policy CP19 is amplified by Appendix 7 of the Core Strategy, the South of the M4 Strategic Development Location Supplementary Planning Document (SPD) and Infrastructure Delivery and Contributions SPD, which addresses the associated infrastructure impacts across the whole Borough. These documents establish a requirement for a sustainable, well designed, mixed use development and make clear that a co-ordinated approach to the development of the SDL will be required to deliver the necessary infrastructure, facilities and services to meet the needs of the expanded community. The development site sits within the development limits as outlined in Policy 1 of Shinfield Parish Neighbourhood Plan
4. The outline consent included an Infrastructure Delivery Plan and S106 legal agreement. The legal agreement secures the coordinated delivery of the infrastructure necessary to support the development and fair share of the SDL wide infrastructure. These included contributions towards off-site infrastructure and services such as roads, education, sports facilities, community facilities and green infrastructure. In addition, it secured on site affordable housing and an affordable housing contribution in accordance with the Core Strategy and Infrastructure Delivery and Contributions Supplementary Planning Document.

*Site background/established principles*

5. The details approved under the outline planning consent established parameters for the development including general site layout including the quantum of development, indicative location of housing, open space, density and general heights of buildings.
6. The outline also established access to the site. The principal access to the site was through the Croft Gardens development to the south of the site which is well advanced on being delivered. Access points to the site were also established on Ryeish Lane and Hyde End Road to provide outgoing vehicle movements from the site along Hyde End Road to the north and incoming movements via Ryeish Lane from the north.
7. In addition to this a further access point was established to the west of the development site from Clairs Green Road for 100 houses which is controlled by condition 40 of the outline planning permission. The current application falls below this threshold. The submitted masterplan utilises these points of access although this will be subject to approval of Traffic Regulation Orders which have been submitted to the council.
8. The outline application also established the principle of an up to three-form entry school for the site, which would be located to the east of the site adjacent to Hyde End Lane. Wokingham Borough Council have elected to construct the school and a financial contribution towards the delivery of this was secured which is proportionate the need generated by the 900 houses approved by the outline approval. The developers have however worked with the council to help secure some of the drop off spaces within the development site and the layout takes into account the

relationship with the school.

9. The current planning application broadly reflects the principles identified above and a Design Code has been submitted for the site. This establishes the overarching principles for the site for areas such as highway widths and design principles for the houses themselves. This will ensure consistency in design approach of the key streets / spaces once the future development parcels are brought forward. The main principles of the design code and how they have been applied for this reserved matters application are further discussed in paragraphs 18-26.

#### *Site Description*

10. This area is located to the east of Spencers Wood. The site is bound to the north by Croft Road, to the west by Hyde End Lane, to the east by Clairs Green Road and to the south by Ryeish Lane. The land is predominantly open countryside and generally serves as pasture. There is a row of houses located on Clares Green Road which would back onto the site and other sporadic development located on Ryeish Lane and Croft Road. The site is characterised by vegetation features which include hedgerows and trees and these create paddock style enclosures. The land is relatively flat although slopes gently up towards the west.

#### *Affordable and specialist housing*

11. Core Strategy Policy CP5, *Housing mix, density and affordability*, amplified by MDD LP Policy TB05: *Housing Mix*, South of the M4 SPD Design Principle 2b, the Infrastructure SPD and the Affordable Housing SPD, requires a mix of tenures, including 35% affordable housing within SDLs.
12. The application proposes 31 affordable dwellings (24 two bedroom units and 7 three bedroom units) on site equating to 8.5%. The S106 pursuant to the outline application secures a 25% off site affordable housing contribution. A higher proportion of the affordable housing was delivered in the Croft Gardens development site and the approach is in accordance with the agreed strategy in the S106 pursuant to the outline application.
13. This will meet the need to secure the 35% affordable housing required across the two development parcels and the affordable housing officer is satisfied with this approach.
14. The location of affordable housing and clustering of it has been raised by representations received. The affordable housing has been designed to be tenure blind (i.e. blend in and not be distinctive in design to the market housing). Furthermore, clustering the units is preferred by the Regional Housing Providers as it helps with the future management of the dwellings. This is an accepted approach nationally. On this basis, the location and distribution of the affordable housing is considered acceptable.
15. It is considered that the application is in accordance with the agreed strategy and will secure the 35% affordable housing requirement. The affordable housing officer is satisfied with this strategy.

#### *Dwelling mix*

16. MDD LP Policy TB05 requires an appropriate housing mix, which reflects a balance between the character of the area and the current and projected needs of

households. The application proposes the dwelling mix outlined in table 1:

**Table 1: Dwelling mix**

Dwelling Type (bedrooms)	Number provided on site	2 Storeys	2.5 Storeys	3 Storeys
1 bed apartment	0	0	0	0
2 bed apartment	103	4	0	99
2 bed house	41	41	0	0
3 bed house	138	63	63	12
4 bed house	68	59	9	0
5 bed house	13	0	13	0

Note: the apartments will be delivered in 9 blocks

17. Officers raise no objections in terms of the dwelling mix but it should also be noted that although officers do ensure that there is an appropriate mix of house types, this is also driven by market conditions. Taking the constraints of the site into account, this approach is considered acceptable.

*Masterplan – Layout*

18. Core Strategy Policies CP1, *Sustainable Development* and CP3, *General Principles for Development* requires high quality design that respects its context. This requirement is amplified by MDD LP Policies CC03, *Green Infrastructure, Trees and Landscaping* and TB21, *Landscape Character* and South of the M4 SPD which requires development proposals to protect and enhance the Borough’s Green Infrastructure, retaining existing trees, hedges and other landscape features and incorporating high quality - ideally native – planting as an integral part of any scheme, within the context of the Council’s Landscape Character Assessment.

19. Core Strategy policy CP19 sets out the concept rational for the design parameters for the South of the M4 SDL and these are outlined in further detail in Appendix 7 of the Core Strategy. This states that:

*Development in a series of locations around the periphery of the three villages is required. This approach should consider the relationship between the current built areas and the open countryside. Opportunities to form new edges to the existing villages exist, allowing a managed transition between urban and rural.*

20. Policy 1 of Shinfield Parish Plan sets out the development limits. These follow those identified in the South of the M4 SPD. The development site is within these limits.

21. Policy 2 sets out general design principles and requires that development is complementary to the built environment surrounding the development site and layouts should acknowledge existing landscape constraints. Policy 6 is concerned with Trees, Hedgerows and Woodland in new development and sets out a landscape led design approach with retention and appropriate buffers to existing important vegetation.

22. Further design guidance is provided by the South of the M4 SPD in section 4. Design principle 3 is concerned with character and states that there should be diversity and distinction in the SDL which should be enhanced through the application of character typologies.
23. It is important to set out the site in the context of the overarching outline planning permission for Spencers Wood and Three Mile Cross. The application parcels forms a significant part of the Spencers Wood planning unit. In order to ensure consistency of design for the entire planning unit, the overarching planning consent established design principles including indicative parameter plans for development limits, density and building heights. Further design guidance was also included within the Design and Access Statement (DAS).
24. To build upon these principles, a design code has been submitted with this reserved matters application. This will ensure a consistent approach for development between the current development parcels.
25. The submitted masterplan retains the principles within the outline planning application and design code. To reflect this, respond to the site constraints and inform diversity of development, the applicant has applied four different character areas to the layout. These are the Village Form for the main spine road, Village Form for the Southern development parcels, Rural Edge for the northern development parcels and Rural Edge for the western development parcels. Within these areas, differing street typologies, building heights and landscaping have been applied to promote variety and also manage vehicle speeds throughout the site. In addition, consideration has been applied to the relationship of the site and the constraints of the surrounding area through the application of these differing character areas. This is shown in figure 1 below.



**Figure 1: Proposed Character Areas**

26. The principle layout of the site has been established by the parameters established within the outline planning application. This identified the key routes through the site and principles identified in the approved DAS have informed street hierarchy, landscaping and drainage features. As these have been established, the layout is considered acceptable.

*Masterplan – Design and appearance*

27. In terms of the detailed design of the buildings, the South of the M4 SPD recommends that the existing built form should be used to inform the design which is echoed by the Shinfield Parish Plan policy 2. The design code sets out the parameters for the site and builds upon the principles established by the design and access statement for the outline planning consent. It should be noted that the parameters are somewhat flexible subject to detailed design. The current application incorporates the Village Form – Spine Road, Village Form – Southern Quarter, Rural Edge – Northern Area and Rural Edge – Western Quarter in the design and these are taken in turn below.

#### *Village Form Spine Road*

28. The spine road is the principal road that runs through the site. In order to define this, regular frontage form of development is proposed with consistent setback of building types. The area would be characterised by taller buildings which are mostly apartments to define the main spine road. The detailed design of these follows on from the Croft Gardens development site to the south (formally referred to as south of Croft Road). This is considered to be a sound and coherent design approach and will allow future users to recognise that this is the main spine road within the development. Building heights in this area are predominantly 3 storeys, which reflects the design theme established at Croft Gardens. Parking for the apartments is set out in parking courts to the rear of the buildings, which decrease the dominance of parking at the front and allows for landscaping in front of the apartments. This will also help to manage on street parking for the bus link, which will ultimately run through the development site.

#### *Village Form – Southern Quarter*

29. Part of the southern Quarter is located adjacent to Croft Road. In order to manage the interface between the main spine road, the applicant is proposing predominantly 2 storey dwellings in this location. These have more diversity in terms of architectural style and a less defined layout than the main spine road with building setbacks varying within the development parcels. Existing landscaping has been well considered in this part of the site which will soften the impact of the built form from the start. The design approach is considered to respect the prevailing character of development on Croft Road and the interface between the site and countryside to the east.

#### *Rural Edge – Western Quarter*

30. The Western Quarter is located adjacent to Clairs Green Road where there is established existing built form. The design approach for this area is to retain the significant existing landscape features where possible and lay the houses out in within the development parcels as defined by the existing hedgerows. The detailing of the buildings is reflective of the existing built form. Building setbacks in this location are reasonably regular which reflects the existing form of development in the area adjacent to the development parcel. Building heights in this area is mostly 2 with some 2.5 storey unit types used to provide variety in the ridge line.

#### *Rural Edge – Northern Area*

31. This area is set within established hedgerows which have been considered in the site layout and the majority of which will be retained. The building line in this location is less consistent than for other parts of the site to respond to the landscape setting and rural edge of the site. Building types here are traditional to respond to the edge of development location and existing built form although some modern elements have been incorporated. Houses types in this location are mainly detached / semi-detached units and occasional terracing. This again is a good response to the setting of this part of the site.

32. Throughout the site, the applicant has provided a good range of housing types and styles ensuring that there is diversity in the built form and a range of housing for future occupants. The application of four character areas outlined above also helps provide variety throughout the site in terms of building types, height and layout. The application of differing design principles for the development parcels should provide a clear and legible neighbourhood for future occupants/users. Furthermore there is

good diversity in the detailing and appearance of the houses using a mixture of materials such as rendering on some house types and brick work details on others.

33. In terms of building heights, the proposal is generally in accordance with the parameters of the outline planning permission. There are however, a limited number of dwellings would exceed the AOD heights for the outline application. These however are generally in response to the drainage constraints of the site with levels raised to ensure suitable drainage channels are incorporated (see paragraphs 86-90 below) and to provide appropriate buffers to landscape features. This only affects 18 of the 363 dwellings and although a couple of these would be 60cm above the AOD heights, the majority of the other 16 are between 10-30cm above the approved parameter plans. Taken as a whole of the 18 units there is an average of 25cm increase above the AOD heights. It is considered that any perceived harm relating to this is minimal and the impact of the increase in building heights are offset by the benefits of the retained vegetation and the drainage strategy. Therefore these are acceptable and when taken into consideration of the site as a whole, are not considered materially harmful to the appearance of the local area.
34. The layout also incorporates two areas of play space within the design. These are located to the north and east of the application site. The location of these is considered acceptable and would provide for good access for the future residents especially when taking into account the proximity of the newly open Croft Gardens play area that has been delivered to the south of the site.
35. The overall design approach for all of the dwellings provides continuity in approach for the Croft Gardens development and is considered to respect the context of the site location and advice provided by the NPPF, policies CP1 and CP3, Policy 2 of the Shinfield Parish Neighbourhood Plan together with the South of the M4 SPD and Supplementary Planning Guidance and is therefore acceptable.

#### *Masterplan – Density*

36. Design principles 3 (a-g) are outlined in the South of the M4 SPD and are concerned with the character of the development, including the density of development. The Core Strategy approach for the South of the M4 SDL in respect to density is outlined in Appendix 7 which indicates an average density of 30-35dph across the SDL.
37. The overall density proposed by the application is 31dph. This falls comfortably in the aims of the policy and the principles established for the outline. In order to achieve this and respond to the site constraints, the applicant has focussed higher density to the centre of the site which would not only help define the principal road through the site but through reducing density towards the edge, has a good transition between the rural edges of the site and existing built form.

#### *Masterplan – Landscaping*

38. The site at present is defined by established hedgerows and a number of trees that form paddock style enclosures. The proposed masterplan has taken these into consideration and incorporated the significant majority of the landscape features into the layout with appropriate buffer-zones. In the majority of cases, housing has been orientated to front and or side onto this and this approach will help soften the development from the outset as well as providing natural surveillance to these areas

which will help with management. A central SUDs pond would define the entrance to the site and a further SUDS pond adjacent to Hyde End Lane would provide a buffer from the development and the countryside.

39. Overall, the majority of the significant hedgerows and trees have been retained and where there are losses, these are either compensated for or these are species not considered significant by the Landscape Officer. Semi mature trees are proposed within the site layout which would help soften the built form and provide a verdant character along the areas of public open space. The Landscape Officer has assessed the planting strategy and is satisfied with the planting at this stage. Further details in respect to planting can be secured by conditions 9-16 of the outline planning consent.

#### *Masterplan conclusion*

40. The design approach taken to inform the layout and appearance of the buildings is considered to respond well to the existing landscape features, context of the site and in respect to the surrounding area. The applicant has strongly demonstrated a comprehensive approach to the delivery of the houses under consideration through the application of the principles established in the design code within the layout and design choices of the units themselves.
41. Overall, it is considered that the proposal would not have a significant detrimental impact on the character of the area and is broadly in accordance with the principles of the relevant planning policies and supplementary planning advice.

#### *Residential amenity: the impact upon existing neighbouring properties*

42. Core Strategy policy CP3 requires that new development should be of a high quality of design that does not cause detriment to the amenities of adjoining land users. Policy 2 of the Shinfield Parish Plan seeks to ensure that new development does not harm existing residential amenity. Separation standards for new residential development are set out in section 4.7 of the Borough Design Guide.

#### *Wilders Grove Farm*

43. This house would front onto plots 615-617. There is however 24m front to front separation which overcomes any significant harm.

#### *Willow Barn*

44. Plot 486 would side onto Willow Barn with 6m separation to the common boundary and 20m to the main dwelling. This relationship is considered acceptable given that views from plot 617 would be oblique.
45. Plot 474 would side onto the rear boundary of Willow Barn. There is 15m separation to the main dwelling house from 467 and no windows are proposed in the side elevation. This is in excess of the 12m recommended by the Borough Design Guide.
46. In respect to plot 467 this would side onto Willow Barn. There is over 25m separation from plot 467 to Willow barn which exceeds the guidance.

#### *Nullis Farmhouse*

47. Plot 466 would back onto Nullis Farmhouse. There is however 25m separation from the rear wall of plot 466 and Nullis Farmhouse.

48. Plot 615 would side onto Nullis Farmhouse. There is however 17m separation between the new house and Nullis Farmhouse.

*73 Clares Green Road (The Cottage)*

49. Plot 513 would side on to the rear of No. 73 Clares Green Road. There would be 17m between the new house and the existing house which is in excess of the 12m flank to rear separation outlined in the BDG.

*79 Clares Green Road*

50. Plot 511-513 would be located some between 12m and 15m away from the common boundary of 79 Clares Green Road. Furthermore the level of separation to the main house is around 21m. This is well in excess of the 12m flank to rear separation outlined in the BDG

51. For plots 509-510, these are set at an oblique angle to the main house at 79 and there is over 40 m separation between the two plots and No.79. This comfortably exceeds the 22m back to back separation outlined in the BDG

*79-89 Clares Green Road*

52. The proposed new houses (plots 501-508) would back/side onto the existing dwellings. There is however over 50m between these and the proposed houses which is in excess of the guidance.

53. For number 89, plot 501 would face towards the bottom of the garden of the existing house however there is over 15m between the house and the garden. This is in excess of a comparable relationship of rear to flank and given that this is not the main amenity area, the relationship is considered acceptable.

*Saxon Close*

54. Number 3-6 Saxon Close would front onto the development site and face towards the main front wall of the proposed dwellings. There is however a minimum of 22m separation from the plots 495-502 which is well in excess of the 10m front to front separation outlined in the BDG.

*Croft Road – Ferndale and Croft Cottage*

55. Plots 732-735 would be located around 25m away from the main front wall of Ferndale and Croft Cottage. The new houses would have a front to front relationship with the existing dwellings. The level of separation is well in excess of the 10m outlined in the BDG.

*Croft Road – Walnut Barn*

56. Plot 766 would back onto Walnut Barn. There is, however, 30m separation to the existing house, which is sufficient to overcome any significant amenity harm.

*The Walnut, Chevin Croft, The Mead*

57. The level of separation from plots 786 and 779 ranges from between 35 – 40 m to the existing residential dwellings. The new houses would side onto the existing dwellings. This level of separation is in excess of the guidance outlined in the BDG.

58. As outlined above the development is in accordance with the separation standards outlined in the Borough Design Guide. Where there is no prescribed standard, a comparable relationship has been applied. Although in certain parts of the site, the

levels would increase, the separation distances achieved helps overcome any harm associated with this. Given these factors and the onsite conditions, it is considered that there would be no significant issues arising in terms of loss of light, overbearing or overlooking. Planning conditions 10-11 will ensure that this relationship remains in the future.

59. Given these relationships and the onsite conditions, it is considered that there would be no significant amenity impacts to existing residents in terms of overlooking, loss of light and overbearing.

*Residential amenity – noise*

60. Whilst it is acknowledged that there would be a greater level of activity in the area arising from the delivery and future occupation of the housing, this would not be to an extent that is unusual in a residential area. As the site has been identified as being suitable for housing, the principle of residential development in this area has been established. Construction activities would be temporary and in order to reduce the impact for the surrounding residents, these can be controlled by conditions 24 of the outline planning permission. It is considered that the proposal would not result in any significant harm to the amenity of the existing residents and is in accordance with policies CP1 and CP3 and supplementary planning guidance.

*Residential amenity: the amenity of future occupants of the development*

61. The Borough Design Guide and MDD LP policy TB07 set out minimum thresholds for private amenity space and internal space standards which new residential development should be assessed against.

*Private amenity space*

62. The Borough Design Guide sets a minimum garden depth of 11m. In terms of the submitted masterplan, the proposed layout complies with these requirements. All houses have sufficient garden depths and flats have private amenity space wherever possible through balconies or defensible space. Overall, with the level of outdoor amenity space is considered to provide acceptable levels of private amenity space and privacy for the future occupants of the dwellings.

*Internal Space Standards*

63. With respect to internal floor space of the units, the Borough Design Guide and Policy TB07 of the MDD LP establish internal space standards for new dwellings, setting standards for both the gross internal area (GIA) and the minimum combined area of the living, kitchen and dining spaces for houses. All the proposed units either meet or exceed the minimum internal space standards and the combined living, kitchen and dining area specified in policy TB07. This will ensure an appropriate standard of living and amenity will be enjoyed by future occupiers of the dwellings.

*Security*

64. The layout takes into account security and all of the communal areas of open space are well overlooked by the proposed housing, as are the areas of parking. The layout also means that there are back-to-back relationships, which restrict access to private amenity space. The layout and design are considered acceptable.

*Noise*

65. Core Strategy Policy CP1 and MDD LP Policy CC06 direct development away from

areas where noise would impact upon amenity and require mitigation where noise cannot be completely avoided.

66. The Environmental Statement submitted for the outline planning permission established that the main sources of noise for the application site Hyde End Lane, Ryeish Lane, Croft Road and Clairs Green Road and the school which will be delivered at a later stage. These are however low sources of noise and any impact can be overcome by appropriate glazing for the houses that are located close to these areas. On this basis there should be no significant impacts to the occupants of the proposed dwellings. In terms of noise within the development, this would not be at a level that is unusual in a residential area.

#### *Sustainable Design and Construction*

67. Core Strategy Policy CP1 requires development to contribute towards the goal of achieving zero carbon development by including on-site renewable energy features and minimising energy and water consumption. MDD LP policies CC04, CC05 and the Sustainable Design and Construction Supplementary Planning Document (May 2010) also emphasise this. Policy 3 of the Shinfield Parish Neighbourhood Plan also echoes these principles.
68. The Design and Access Statement makes a commitment towards sustainable design and construction. In order to ensure that this commitment is achieved and in accordance with the requirements part L of the building regulations. The council seeks both fabric first approach (such as housing insulation and low energy appliances) and on site renewables in addition to water resilience measures and as such, it is not necessary to duplicate by way of condition.
69. As part of delivering sustainable development on the site, the Council expects measures to improve water resilience and reduce water consumption to be included within the development. In the case of water, this can be achieved by the inclusion of aerated taps, shaped low capacity baths, dual flush toilets, low flow rate showers and the provision of water butts for water recycling. Again building regulations covers this aspect and a separate condition is not required.

#### *Access and movement*

70. The outline planning permission for Spencers Wood and Three Mile Cross established the access points to the site and was accompanied with a full Transport Assessment. This included modelling the impacts of the development by using the Wokingham Strategic Transport Model which included a review of this development site, the South of the M4 SDL and the wider Core Strategy development proposals. It was demonstrated that there would be no significant harm caused by additional houses to both local and the wider transport networks, subject to the delivery of the Transport Interventions that were identified and secured through a legal process. The current reserved matters application does not deviate from the established access points approved under the outline planning application.
71. Policy 4 of the Shinfield Parish Neighbourhood Plan requires development to provide good accessibility by car, cycle and foot and ensure highway safety. Encouragement of use of public transport is also promoted. Policy 5 of the Shinfield Parish Neighbourhood Plan sets out parking standards. These should be in line with WBC standards and well set out with good surveillance to parking courts.

### *Site Access*

72. The accesses to the site are fairly complex and there is a good emphasis on encouraging walking, cycling and public transport to and from the site. General vehicular access is available from three directions::
- i. Southwards onto Hyde End Lane across Croft Road and through Croft Gardens. This connects to the main spine road through the site, which is Primary Street. This spine road was subject to a detailed planning application (ref number 170239), which was approved in May 2017. This application included the details of the Primary Street between Croft Road and Hyde End Lane that also links to the Sustainable Link that connects to Shinfield for buses, cyclists and pedestrians;
  - ii. Northward to Hyde End Lane. This is a one way street northbound from the spine road along the existing Hyde End Lane and southbound is a one way street along the existing Ryeish Green Lane before connecting with a new two way street with the spine road;
  - iii. Westwards access is provided for 70 dwellings onto Clares Green Road. These dwellings are separated from the rest of the site for vehicular access, the level of units accessing this parcel was determined at the outline planning stage.

### *Access and movement - Site layout*

73. As outlined above, the use of character areas ensures that there is a variety of street hierarchy proposed for the site. The principal accesses to the site from the south would be off Hyde End Road via Croft Gardens and from the north off Hyde End Lane. The Primary Street has been designed to be 6.1m wide to allow for two buses to pass simultaneously. In order to ensure good public transport to the site, the South of M4 Public Transport Strategy has buses running along this Primary Street. There is a pair of bus stops central to the site that ensures good public transport connectivity for encouraging users. Footpaths along the spine road are 2m wide on one side and 3m on the other side to cater for both pedestrians and cyclists.
74. There is a Secondary Street that loops around the proposed Primary School site and provides the northern access for the site. The public transport strategy does not require buses to run through this part of the site and accordingly the road width has been designed to be 5.5m wide where it is two way and reduces in width on a section using part of the existing Hyde End Road where it is one way. The one way sections are the existing links of Hyde End Lane and Ryeish Green Lane which are partially within the red line of the planning application. Whilst these existing roads are narrow and bound on either side by hedgerows, footway provision is available on at least one side of these lanes. On the section of Hyde End Lane between the spine road and Ryeish Green Lane, adjacent to the school site, this footway will be provided on the west side of the hedgerow. Meanwhile where possible, on the new highway section there will be footways on both sides of the road.
75. The side Streets and Rural Lanes would be served by a mixture highway with footways on one side, shared surfaces and private drives. These roads tend to be short in length and have reasonably tight corners which will ensure low vehicle speeds within these zones. They have been checked to ensure that vehicle tracking is acceptable for refuse vehicles. The road widths within these lower key

roads vary between a minimum of 4.5m to 5m, with some of the shared space roads being wider due to the combined road and footpath space.

*Access and movement*

76. In line with Core Strategy Policy CP6, Managing Travel Demand and MDDLDP Policy CC07: Parking, condition 40 requires reserved matters to incorporate car and motorcycle parking in line with the Council's standards. The standards require allocated parking to be supplemented with unallocated parking.
77. The application car parking numbers are summarised in the Table 2 below. This shows a gross total of 965 parking spaces, over 2.6 per dwelling (2.66), which also includes garages and visitor spaces. There are 630 allocated spaces, 124 visitor spaces and 209 garages. Some 58% of dwellings (209) have a garage.

	<b>nos</b>	<b>ratio of 363</b>
<b>Total Visitor</b>	<b>124</b>	0.34
<b>Total Allocated</b>	<b>630</b>	1.74
<b>Total Garages</b>	<b>209</b>	0.58
<b>Total Allocate &amp; Garages</b>	<b>839</b>	2.31
<b>Total Visitor, allocated &amp; garages</b>	<b>963</b>	2.66

**Table 2: Summary of Car Parking Provision**

	<b>With Visitors</b>		<b>Allocated (excluding visitor)</b>	
	<b>spaces</b>	<b>ave / unit</b>	<b>spaces</b>	<b>ave/unit</b>
Total spaces: allocated, garages & visitor	<b>963</b>	<b>2.65</b>	<b>839</b>	<b>2.31</b>
Total spaces with 50% for garages	<b>859</b>	<b>2.37</b>	<b>735</b>	<b>2.02</b>
Total spaces with 0% for garages	<b>754</b>	<b>2.08</b>	<b>630</b>	<b>1.74</b>

**Table 3: Further Analysis of Car Parking Provision**

78. Through working with the applicant, a level of car parking has been achieved to accord with guidance contained within the Council's adopted Parking Standards Study Report.
79. The parking management strategy condition, secured under the consent of the outline planning permission, provides added support to help address any future associated parking problems and difficulties moving forward.
80. The Council are promoting more electric charging points in new developments as housing without this facility can be a significant barrier to electric car ownership. Dwellings with a garage can assume to have an electric supply, but non-garaged units should have access to facilities. It is recommended that there is a minimum of 10% active electric charging provision and 20% passive provision. Active means a direct connection is provided and passive means that a cable end is left for later connection. The applicant is providing a single passive charging point to each flatted parking court, which would equate to about 5%. However, with a passive point in each parking court it is considered reasonably straight forward to extend the charging provision to additional spaces and therefore this provision should be welcomed.

#### *Access and Movement – Pedestrian & Cycle Access*

81. The layout provides for pedestrian access with 2m footways on both sides of the main access roads within the site and 3m combined footway/cycleway on the main spine road. The side roads are either secondary or tertiary streets which include footways that connect to other Spencers Wood phases, or shared surface for the minor accesses. Connectivity is an important feature of Spencers Wood site, with vehicle cul-de-sacs having through routes for pedestrians. Safe routes to school are identified, which in the early stages of the development will rely for some users, on a route out of the site along the Sustainable Transport Link connecting to the Primary School in Shinfield West. In the longer term when the new primary school is open within this site, pedestrian and cycle routes will connect to the Primary School on the north eastern part of the site. For access to the nearest Secondary School at Oakbank there is a short walk or cycle ride along existing and on new footpaths being delivered.
82. There are a number of green lanes being adapted for pedestrian, cycle and access use only. These are Hyde End Lane, south of the spine road and Ryeish Lane between Willow Barn and the secondary street that links back to the spine road. In the longer term part of the one-way section of Croft Road may also form part of a vehicle closure to through traffic, which would provide further improvements for pedestrians and cycles. These changes to the use of the existing lanes will be delivered via a series of Traffic Regulations Orders (TROs) which are a separate process to the planning permission. The works and changes associated with the TRO changes will be carefully phased to ensure development and construction traffic is not routing along them.

#### *Access and Movement - Cycle storage*

83. Cycle parking is provided within sheds for houses where there is no garage provision and cycle parking stores for flats. Where dwellings have a garage, they comply with the minimum dimensions for cycle parking of 3m by 7m. All dwellings have been provided with sufficient space for cycle storage / parking in accordance with the above requirements except for one block of flats (plots 530-542) and this is subject to a planning condition to submit additional details. Cycle visitor space 'Sheffield Stands' are provided near the entrance to each block of flats.

#### *Access and Movement - Access to public transport*

84. A South of M4 Public Transport Strategy is expected to provide up to a 15 minute each way frequency buses routing through the site. This frequency of service combined with high quality bus stop infrastructure and the bus priority, that includes the bus only link between Spencers Wood and Shinfield West, is expected to make the bus an attractive service for many destinations. This service is already in operation and provides service to the existing development south of croft road.
85. Before the bus only link between Shinfield west and Spencers Wood is completed, an interim bus strategy has been identified. Initially, bus services will be accessed from Croft Gardens south of Croft Road and the stop is within a 400m walk. This is currently an hourly service that is expected to increase to 2 per hour during following phases.

#### *Flooding and Drainage*

86. Core Strategy Policy CP1 and MDDL Policies CC09 and CC10 establish that new

development should avoid increasing and where possible reduce flood risk (from all sources) by first developing in areas with lowest flood risk, carrying out a Flood Risk Assessment (FRA) where required and managing surface water in a sustainable manner. These requirements are reinforced by the South of the M4 SPD Design Principle 1c(ii) which requires provision of a comprehensive system for water management, which takes account of existing features and includes proposals for effective sustainable urban drainage (SUDS), measures to avoid flood risk and new ponds. Policy 8 of the Shinfield Parish Neighbourhood Plan echoes these principles and looks to retain existing watercourses in new development.

87. The site is entirely within Flood Zone 1 where the risk of flooding is low and all uses, including more vulnerable residential uses and less vulnerable uses (the proposed school, shops and community uses) are appropriate. Furthermore, the site was allocated in the Core Strategy, which was subject to a Strategic Flood Risk Assessment.
88. The outline application was supported by a Flood Risk Assessments which assessed the impact of flooding on the application site. This identified flood risks across the whole site and propose measures to manage surface water runoff including the 1 in 30 year and 1 in 100 year storm events (+30% for climate change) for the areas of residential development.
89. In order to ensure that the increase in surface water runoff associated with the additional built form is managed, a strategic approach to drainage is proposed. This included 2 SUDS ponds in the strategic greenspace. Additional drainage management is also proposed with the formation of two new ditches to the west and east of the site. These will not only attenuate storm water on site and control the discharge but will also provide landscaping features.
90. The Flood Risk Assessment and Drainage strategy propose measures on site have been assessed by the council's Drainage Officer. They have raised no objection to the proposed package of measures in terms of the impact to existing neighbouring residents and future occupiers of the site. On this basis the proposals are considered acceptable.

#### *Thames Basin Heaths Special Protection Area*

91. The Thames Basin Heaths Special Protection Area (SPA) was designated under European Directive due to its importance for heathland bird species. Core Strategy policy CP8 establishes that new residential development within a 7km zone of influence is likely to contribute to a significant impact upon the integrity of the SPA. The South of the M4 SDL falls within this zone of influence and, in accordance with Core Strategy policy CP17 and Design Principle 1c (vi), mitigation in the form of Suitable Alternative Natural Greenspace (SANG) is being provided.
92. The development will be mitigated by the provision of the SANG strategy approved under the outline planning consent which will provide three areas of SANG at Five Acre Field, The Ridge and Mays Farm. Full planning permission has been approved for these SANG areas and as such this meets the Natural England's '*Guidelines for the Creation of Suitable Alternative Natural Greenspace*' (SANG) (2008) in terms of having all the essential features required to attract recreational users away from the SPA. Natural England and the Biodiversity Officer are satisfied with this approach.

### *Ecology*

93. Core Strategy Policy CP7, carried forward by MDD LP Policy TB23, requires appropriate protection of species and habitats of conservation value. Policy 7 of the Shinfield Parish Neighbourhood Plan seeks mitigation and measures to enhance biodiversity. Design Principle 1b of the South of the M4 SPD (i-ii) is concerned with protection of ecological habitat and biodiversity features, together with mitigation of any impacts that do arise. This demonstrates that the site layout has considered impacts on ecological permeability. In addition no advisee impacts are envisaged to flora and fauna and a detailed mitigation strategy has been provided.
94. In terms of existing vegetation, the applicant has demonstrated that there would be an adequate buffer zone from the proposed development to these features. The Biodiversity Officer has assessed the application and is satisfied that there would no significant impact on ecology.

### *Heritage*

95. Policy TB24 of MDD LP seeks to ensure that development conserves and, where possible enhances the important character and special architectural or historic interest of listed buildings. Nullis Barn is located adjacent to the application site which is Grade II listed. The Conservation Officer has assessed the application and raises no objection to the impact on the listed building and setting although has requested further landscaping which can be secured by condition 9-16 of the outline planning consent.

### *Archaeology*

96. Core Strategy Policy CP3 and MDD LP Policy TB25 require the archaeological impact of development to be taken into consideration. Condition 43 relating to archaeology was attached to the outline permission for a written scheme of investigation prior to the commencement of development and this approach is supported by the Council's Archaeological Officer.

## **CONCLUSION**

The reserved matters are consistent with the principles and parameters established by the outline planning permission, which themselves reflect the Council's adopted policies and those within the Shinfield Parish Neighbourhood Plan and guidance for development within the South of the M4 SDL. It is considered that the applications will deliver high quality development in accordance with the Council's spatial strategy and vision and therefore can be recommended for approval.

## **CONTACT DETAILS**

<b>Service</b>	<b>Telephone</b>	<b>Email</b>
Development Management and Regulatory Services	0118 974 6428 / 6429	development.control@wokingham.gov.uk



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170010

Land north of Croft Road, west of Hyde End Lane,  
north and south of Ryeish Lane, East of Clares Green Road, Spencers Wood  
Proposal Reserved Matters application pursuant to Outline  
Planning Consent (O/2013/0346) for 363 dwellings including internal access roads, garages, parking  
spaces, SuDS attenuation, open space, Play Areas and associated landscaping enhancements.  
Planning Officer Chris Howard Comments by 10 February 2017

#### Comments

Shinfield Parish Council requests that the developer confirms that the proposals within this Reserved Matters application meet the requirements of the Shinfield Neighbourhood Development Plan. It will be deemed acceptable if:

- The proposal for the secondary street, currently showing as 4.8m wide and designated as a two-way bus route, and principal access route to the school, and to which Shinfield Parish Council strongly objects, should be redesigned as a primary street. The current design is in abject denial of the current situation experienced at virtually every single primary school within the borough, where school drop-off and pick up parking creates significant additional on-street parking pressure on local roads [SPC NHDP Policy 4].
  - Measures to restrict on-pavement and on-verge parking within the designated bus-route be incorporated. The Parish Council is particularly concerned about the siting of apartments along the secondary road, many of which will only be provided with 1 allocated parking space, and which are likely to lead to significant on-street parking along the narrowest section of road, which is proposed to be a two-way bus route [SPC NHDP Policies 4 and 5].
  - This development currently shows a significant prevalence of tandem parking, which will lead to unwanted on-street parking, traffic congestion, and an increase in the incidents of vehicles reversing off driveways, which could be extremely dangerous if there is a tendency for cars to park on the street.
  - There is a provision of a safe crossing point between the school site and the play area [SPC NHDP Policy 4]
  - Garages comply with the Shinfield Neighbourhood Development Plan, and be 3m by 7m, with sufficient door widths, in order to accommodate cycles and larger vehicles [SPC NHDP Policy 5].
  - The provision of unallocated parking spaces is sufficient to meet the recognised demand for the proposed housing mix.
  - There is an appropriate range of design variety to be in-keeping with the surrounding housing supply [SPC NHDP Policy 2]
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# Agenda Item 18.

<b>Development Management Ref No</b>	<b>No weeks on day of committee</b>	<b>Parish</b>	<b>Ward</b>	<b>Listed by:</b>
170618	EXT	Wokingham	Emmbrook / Norreys	Major

**Applicant** Linden Homes  
**Location** Land at Matthewsgreen Farm, Matthewsgreen Road, Wokingham  
**Proposal** Reserved Matters application pursuant to Outline planning consent O/2014/2242 (outline consent for approximately 760 dwellings and associated infrastructure) for Phase 3 of the development comprising 74 dwellings, associated amenity space, access, garaging and parking, internal roads, pathways, drainage and associated landscaping and provision of a section of the streamside recreational park. Appearance, landscaping, layout and scale to be considered.  
**Type** Reserved Matters for Major Application  
**PS Category** 1  
**Officer** Sophie Morris

**FOR CONSIDERATION BY** Planning Committee on 12<sup>th</sup> July 2017  
**REPORT PREPARED BY** Delivery Programme Director

## **SUMMARY**

The application relates to the Matthewsgreen Farm development site; a site comprising 34 hectares in total and situated approximately 2km to the northwest of Wokingham town centre and south of the A329(M). The application site sits within the development Plan allocated North Wokingham Strategic Development Location (SDL).

Outline planning permission for approximately 760 dwellings, a primary school, a neighbourhood centre, community facilities, a section of the Northern Distributor Road and associated works including open space was granted under planning application O/2014/2242 in March 2015. At the time, it determined the planning application, WBC accepted and established the principle and quantum of development on the site. WBC has already approved Reserved Matters for 255 of the 760 dwellings (phases 1, 2a and 2b) and for the Northern Distributor Road (NDR) running through the site.

The current application is a reserved matters application submitted pursuant to the original outline consent for phase 3 of the development comprising 74 dwellings. This is reduced from the original submission of 93 units. The purpose of this application is to provide further detail in respect of the layout, scale, appearance, landscaping and access. It should be noted that the application site has been revised to exclude the eastern parcel of the site, which is the area surrounding the proposed local neighbourhood centre. Officers consider it is necessary to do this to ensure that the development does not prejudice the centre from coming forward.

The development is well designed having regard to the constraints and requirements of the site. It would not cause detrimental impact upon either the character of the area or the amenity of existing residents beyond which was considered acceptable at the outline stage. The proposal would also provide for an adequate level of amenity for the future occupants of the dwellings without detrimental impact on ecology, flood risk, traffic and highway safety.

The reserved matters are similar to the Phase 2b scheme, and do not substantially deviate from the principles and parameters established by the outline planning permission and the Council's adopted policies and guidance in terms of scale, design and appearance. The development would provide high quality development with appropriate levels of parking and amenity for future occupiers of the development. This is a sustainable development that offers substantial public benefit in meeting the housing needs of the community and delivering on Wokingham's development aspirations for the North Wokingham SDL. The application will deliver high quality development in accordance with the Council's spatial strategy and there are no other material planning considerations of significant weight that would dictate that the application should be refused. Officers are therefore recommending the application for approval, subject to the conditions listed.

#### **PLANNING STATUS**

- Within North Wokingham Strategic Development Area
- 7KM Thames Basin Heath SPA
- Archaeological Interest
- Flood Zones 1, 2 and 3 (mainly 1)
- Minerals Consultation Zone
- Contaminated Land Zone
- Ground Water Protection Zone
- Landfill Gas Protection Zone
- Minerals Consultation Zone

#### **RECOMMENDATION**

APPROVE RESERVED MATTERS subject to the following conditions:

##### **Plans**

1. This permission is in respect of plan no's listed below. The development hereby permitted shall be carried out in accordance with the approved drawings and documents listed unless otherwise agreed pursuant to the requirements of the attached conditions or otherwise agreed in writing by the Local Planning Authority.  
*Plan numbers being finalised to be included on Members Update*  
*Reason: To ensure that the development is carried out in accordance with the application form and associated details hereby approved.*

##### **Cycle Parking**

2. No dwelling house shall be occupied until the cycle parking serving it has been provided in accordance with the details of such hereby approved. The cycle parking shall be thereafter permanently retained.  
*Reason: In order to ensure the development contributes towards achieving a sustainable transport system and to provide parking for cycles in accordance with Wokingham Borough Core Strategy Policies CP1 and CP6, the Parking Standards Study within the Borough Design Guide 2010 and the North Wokingham Development Location Supplementary Planning Document (October 2011) and CC07 of the Managing Development Delivery Local Plan (Feb 2014).*

##### **Highways**

3. Notwithstanding the approved plans, the primary access roads from the NDR

serving the development hereby approved shall be constructed with a minimum carriageway width of 5m, unless otherwise agreed in writing with the Local Planning Authority.

*Reason: In the interests of highway safety and convenience. Relevant Policy: Core Strategy policy CP6.*

4. No dwelling shall be occupied until the vehicular accesses, driveways, parking and turning areas to serve it, including the unallocated spaces, have been provided in accordance with the plans hereby approved. The vehicle parking and turning spaces shall be retained and maintained in accordance with the approved details and the parking spaces shall remain available for the parking of vehicles at all times and the turning spaces shall not be used for any other purpose other than vehicle turning.

*Reason: To provide adequate off-street vehicle parking and turning space and to allow vehicles to enter and leave the site in a forward gear in the interests of road safety and convenience and providing a functional, accessible and safe development and in the interests of amenity. Relevant policy: Core Strategy policies CP3 & CP6 and Managing Development Delivery Local Plan policy CC07.*

5. Prior to commencement of development, a revised car parking layout shall be submitted to and approved in writing by the Local Planning Authority. The approved car parking layout, including any garages, visitor and unallocated parking bays must be constructed in accordance with approved plans and retained for such purposes thereafter.

*Reason: To ensure adequate on-site parking provision in the interests of highway safety, convenience and amenity in accordance with Wokingham Borough Core Strategy Policies CP1, CP6 and CP21, CC07 of the Managing Development Delivery Local Plan (Feb 2014), the Parking Standards Study within the Borough Design Guide 2010 and the North Wokingham Strategic Development Location Supplementary Planning Document (October 2011).*

#### **Highway adoption plan**

6. Prior to commencement of development, a highways adoption plan, to include all adoptable service and maintenance margins shall be submitted to and approved in writing by the Local Planning Authority. The development shall be carried out in accordance with the approved details and maintained thereafter.

*Reason: To ensure that the estate is constructed to a standard that would be suitable for adoption as publicly maintainable highway, in the interests of providing a functional, accessible and safe development in accordance with Wokingham Core Strategy Policies CP1, CP6, and CC07 of the Managing Development Delivery Local Plan (Feb 2014).*

#### **Visibility splays**

7. Prior to the commencement of the development, there shall be submitted to and approved in writing by the Local Planning Authority, details of the proposed visibility splays for all vehicular accesses, pedestrian accesses, driveways and cycleways. The visibility splays shall be retained in accordance with the approved details and used for no other purpose and the land within the visibility splays shall be maintained clear of any visual obstruction exceeding 0.6 metres in height at all times.

*Reason: In the interests of highway safety and convenience. Relevant policy: Core Strategy policies CP3 & CP6.*

### **Highway construction details**

8. Prior to the commencement of development, full details of the construction of roads and footways, including phasing, levels, widths, construction materials, depths of construction, surface water drainage, shall be submitted to and approved in writing by the Local Planning Authority. The roads and footways shall be constructed in accordance with the approved details and relevant phases to road base level before the development of the relevant phase is occupied and the final wearing course will be provided within 3 months of occupation of that phase, unless otherwise agreed in writing by the local planning authority.

*Reason: To ensure that roads and footpaths are constructed to a standard that would be suitable for adoption as publicly maintainable highway, in the interests of providing a functional, accessible and safe development. Relevant policy: Core Strategy policies CP3 & CP6.*

9. Prior to occupation of the development hereby approved, full details of the construction of 'Road 24', including levels, widths, construction materials, depths of construction, surface water drainage, shall be submitted to and approved in writing by the Local Planning Authority. The road shall be constructed in accordance with the approved details and to road base level before the development is occupied and the final wearing course will be provided within 3 months of occupation, unless otherwise agreed in writing by the local planning authority.

*Reason: To ensure that roads and footpaths are constructed to a standard that would be suitable for adoption as publicly maintainable highway, in the interests of providing a functional, accessible and safe development. Relevant policy: Core Strategy policies CP3 & CP6.*

### **Sustainability**

10. The water butts as detailed within the submitted '200 litre Water Butt Specification' dated August 2016 and space for composting serving the dwellings, as detailed in Section 8 of the submitted Design Report (Dated February 2017), shall be provided prior to the occupation of the relevant dwelling.

*Reason: To reduce refuse and enable the efficient use of water in accordance with NPPF, Wokingham Borough Core Strategy Policy CP1 and the Managing Development Delivery Local Plan Policy CC04.*

11. The measures to reduce water consumption hereby approved, as set out in the Sustainability Statement (Linden, February 2017), shall be implemented in accordance with the approved details before first occupation of the relevant building.

*Reason: To reduce water consumption accordance with Wokingham Borough Core Strategy Policy CP1 and the Managing Development Delivery Local Plan Policy CC04.*

12. The internal and external spaces for the storage of refuse and recyclable materials for the dwellings hereby approved shall be provided prior to occupation of the relevant building and retained thereafter.

*Reason: To ensure that adequate provision is made for the storage of recyclables in accordance with Wokingham Borough Core Strategy Policy CP1 and CC04 of the Managing Development Delivery Local Plan (Feb 2014), the Sustainable Design and Construction Supplementary Planning Document and the North Wokingham Strategic Development Location Supplementary Planning Document (October*

2011).

### **Informatives**

1. This permission should be read in conjunction with the outline planning permission and associated Section 106 legal agreement.
2. You are informed that this permission here relates only to the Reserved Matters submitted pursuant to condition 2 of the Outline Permission. It does not convey any written approval from the Local Planning Authority as may be required for any other of the conditions of the outline permission; details for which shall need to be submitted separately to and approved in writing by the Local Planning Authority.
3. The applicant is advised that the Council seeks that employers or developers within the borough commit to using local labour / contractors where possible. This should include:
  - Advertisement of jobs within local recruitment agencies / job centres;
  - Recruitment and training of residents from the local area;
  - Seek tender of local suppliers or contractors for work.
4. The applicant is informed that parking will need to be restricted along the main routes (Northern Distributor Road and bus route) and on turning heads.
5. Work on Highway - The Corporate Head of Environment at the Council Offices, Shute End, Wokingham should be contacted for the approval of the access construction details before any work is carried out within the highway. This planning permission does NOT authorise the construction of such an access.
6. Mud on Road - Adequate precautions shall be taken during the construction period to prevent the deposit of mud and similar debris on adjacent highways. For further information contact Corporate Head of Environment on tel: 0118 974 6302.
7. Highway Adoption - If it is the developer's intention to request the Council, as local highway authority, to adopt the proposed access roads etc. as highway maintainable at public expense, then full engineering details must be agreed with the Corporate Head of Environment at the Council Offices, Shute End, Wokingham. The developer is strongly advised not to commence development until such details have been approved in writing and a legal agreement is made with the Council under S38 of the Highways Act 1980.
8. Highway Management - Any works/events carried out either by, or at the behest of, the developer, whether they are located on, or affecting a prospectively maintainable highway, as defined under Section 87 of the New Roads and Street Works Act 1991, or on or affecting the public highway, shall be coordinated under the requirements of the New Roads and Street Works Act 1991 and the Traffic management Act 2004 and licensed accordingly in order to secure the expeditious movement of traffic by minimising disruption to users of the highway network in Wokingham.
9. Utilities - Any works or events commissioned by the developer and particularly those involving the connection of any utility to the site, shall be co-ordinated by them in liaison with Wokingham Borough Council's Street Works Team, (telephone 01189

746302). This must take place at least three month in advance of the works and particularly to ensure that statutory undertaker connections/supplies to the site are coordinated to take place wherever possible at the same time.

10. Noise - The attention of the applicant is drawn to the requirements of Section 60 of the Control of Pollution Act 1974 in respect of the minimisation of noise on construction and demolition sites. Application, under Section 61 of the Act, for prior consent to the works, can be made to the Environmental Health and Licensing Manager.

11. You are advised that the permitted development rights of the properties hereby approved have been restricted by condition 7 and 30 of the outline permission.

### RELEVANT PLANNING HISTORY

**O/2014/2242:** Outline application for a phased development of up to 760 dwellings including 60 units of assisted living homes / older person accommodation a local centre (including retail) a primary school community facilities and associated areas of open space and drainage/ attenuation parking etc. Accesses from Twyford Road, Matthewsgreen Road and Toutley Road (Matters reserved – layout, landscaping, scale, appearance) - Approved March 2015.

**150093:** Reserved Matters for Phase 1 (100 units) - Approved Oct 2015

**152649:** Reserved Matters for NDR – Approved March 2016

**160765:** Reserved Matters for Phase 2a (82 units) – Approved July 2016

**162140:** Reserved Matters for Phase 2b (73 dwellings) – Approved November 2016

**F/2014/1216:** Old Forest Road SANG – approved September 2014

### SUMMARY INFORMATION

RM Site Area	2.78 ha
Existing units	None
Proposed units	74
Number of bedrooms per unit	3 x 1bed; 12 x 2bed; 29 x 3bed; 28 x 4bed; 2 x 5bed
Proposed density - dwellings/hectare	27dph (32dph on 'developable area' excluding open space)
Number of affordable units proposed	0 (0%)
Public Open Space proposed	0.5ha
Proposed parking spaces	168 – 2.3 per dwelling

### CONSULTATION RESPONSES

WBC Waste Services	No objection
WBC Highways	No objection
WBC Environmental Health	No comment
WBC Drainage	No objection

WBC Public Rights of Way	No comment
WBC Landscape Architect	No objection
WBC Affordable Housing	No objection
Berkshire Archaeology	No response. However, response to Phase 2b stated that no further archaeological work is required with the Phase 3 area.
Thames Water	No objection
SGN	No comments received
Berks, Bucks and Oxon	No comments received
Wildlife Trust	

## LOCAL REPRESENTATIONS

Letters of notification were sent out to 138 neighbouring property addresses. The application was also advertised by way of site notice and press advert.

### Responses

#### *Local Residents*

One representation has been received in response to the application consultation. It raises concerns about the loss of trees on the site and the need for a programme of replacements.[See paragraphs 38-40]

*Wokingham Town Council Comments* (see appendix for full comments) [Officer comment in square brackets]:

- Welcome the provision of footways adjacent to most of the roadways in the development;
- The three storey blocks of apartments and (in particular Block B) are of an inappropriate scale and mass compared to the surrounding buildings. The roof space gives the appearance of at least a 4 storey building making the apartments overpowering and out of proportion; [three storey elements were agreed as part of outline parameters; see paragraphs 18 -23 of report]
- The rear elevation of Block B is immensely dominating having a huge area of blank wall with no windows; [design and appearance not considered unacceptable; see paras 24-26]
- This development does not provide the required level of affordable housing;[see paragraphs 29-32 regarding affordable housing]
- Cramped parking spaces between the flank walls of the properties; [these spaces are in accordance with standards and similar in width to those within garages]
- The central area of the site appears to have little visitor/unallocated parking; and
- The road widths are inadequate.[ the road widths are acceptable – the distribution of parking spaces has been amended since the original submission and there are enough spaces serving individual roads and clusters of dwellings]

*Emmbrook Residents' Association comments* [Officer comment in brackets]:

- Noise - this development will be impacted by noise from the NDR and the business estate on its northwest boundary. [Noise mitigation for the dwellings will be detailed, considered and implemented pursuant to condition 51 of the outline consent and does not require further consideration under this Reserved Matters application].
- Layout - The layout results in a development dominated by built form and hard landscaping, which has an adverse impact on the area available for both private and communal amenity space within the built area. This is shown by the irregular

shape of some of the gardens which renders some of their areas of very little use; examples here are plots 83, 92, 117, 133 and 157. [See design section of the assessment.

- Inappropriate and insufficient amenity space for the apartments.[See paragraph 35 of the report]
- Whilst the parking complies with standards, there is a need for deviation from the guidance to increase numbers. [The application complies with current parking standards and there is no reason to require additional provision in the circumstance].

## **PLANNING POLICY**

National Planning Policy Framework (NPPF)

Adopted Core Strategy: CP1, CP2, CP3, CP4, CP5, CP6, CP7, CP8, CP9, CP10, CP11, CP13, CP14, CP15, CP17, CP20 and Appendix 7.

Managing Development Delivery Development Plan Document (MDD Local Plan) adopted February 2014: CC01, CC02, CC03, CC04, CC05, CC06, CC07, CC08, CC09, CC10, TB05, TB07, TB08, TB21, TB23, TB25 and SAL05.  
Appendix 2 (Car Parking Standards).

North Wokingham Strategic Development Location SPD adopted October 2011.  
Infrastructure Delivery and Contributions SPD for the Strategic Development Locations adopted October 2011.  
Affordable Housing SPD adopted June 2011.  
Sustainable Design and Construction SPD adopted 2010

Wokingham Borough Council Design Guide: Borough Guide Design SPD June 2012  
Affordable Housing SPD adopted June 2011.

## **PLANNING ISSUES**

### **Application Site**

1. The application site forms part of the Matthewsgreen Farm development site, which itself forms part of a larger area designated under the Wokingham Borough Core Strategy as the North Wokingham Strategic Development Location (SDL). The Matthewsgreen Farm site comprises approximately 34 hectares in total and is situated approximately 2km to the northwest of Wokingham town centre and south of the A329(M). The site is bounded to the south by Matthewsgreen Road, by Toutley Road to the west, by Twyford Road to the east, and by the Ashridge Stream watercourse to the north.
2. The site previously comprised mainly agricultural land, consisting of open fields, ditches, hedgerows and a mixture of trees in terms of age and type. There is also a small existing commercial operation located towards the southern boundary along Matthewsgreen Road.
3. Outline planning permission for the phased delivery of the Matthewsgreen Farm development (for approximately 760 dwellings and associated infrastructure) has

previously been granted under application O/2014/2242. Reserved Matters planning permission has subsequently been granted for phases 1, 2a and 2b and for the NDR. Development is progressing apace and Phase 1 now has around 30 occupations. The development is also served by a SANG provided to the northern side of Old Forest Road which was granted full planning permission in 2014 and is now open to the public.

### **Application Proposals**

4. Reserved Matters planning permission is sought for phase 3 of the development; which is located towards the north west of the development, in between phase 2b (on Toutley Road) and the proposed location of the local centre and primary school. This application is for 74 dwellings with access from the Northern Distributor Road (NDR), incorporating associated internal access roads, parking, landscaping and open space, footpaths and Sustainable Urban Drainage (SuDS).
5. The proposal has been amended since the original submission, which involves a reduction in the extent of the red line boundary of the site. The red line boundary originally included land extending up to Road 24 in the north east corner of the site as well as further along the NDR boundary to the south east of the site. However, the neighbourhood centre for the development will adjoin the Phase 3 boundary to the east, and has not as yet been submitted for reserved matters approval. Therefore, in order to ensure that the residential layout for Phase 3 does not prejudice the viability of the neighbourhood centre or the level of associated serving/parking, the immediately adjoining land which is located within the Phase 3 development area, has been removed from the current proposals. Accordingly, the number of units proposed has been reduced from 93 to 74. The area which has been removed from the current proposals included 19 dwellings, 6 of which are affordable housing units. This is discussed in more detail in paragraphs 29 to 32.
6. This RMA seeks approval of appearance, landscape, layout and scale in accordance with Condition 2 from the outline permission. The site access from the NDR has been approved in association with the now part implemented Reserved Matters consent for the NDR.
7. A variety of housing typologies are proposed, but are predominantly semi-detached and detached houses, as well as two three-storey apartment buildings. The buildings would range from 1 to 3 storeys.

### **Principle of development**

8. The principle of development has been established by outline planning permission ref: O/2014/2242 and forms part of the 1,500 houses allocated within North Wokingham (Core Strategy Policy CP20).
9. The outline consent was subject to an Infrastructure Delivery Plan and s106 legal agreement, which secured the coordinated delivery of the infrastructure necessary to support the SDL wide development. This included requirements for on-site provision (affordable housing, the NDR, the primary school, the community centre, the local centre and the open spaces) together with contributions towards off-site infrastructure and services such as roads, education, sports facilities and health services. The Old Forest Road SANG, approved under application F/2014/1216,

has been constructed to serve the recreation requirements of the development and thus mitigate its impact upon the Thames Basin Special Protection Area. The SANG is now open.

10. Whilst detailed matters of layout, appearance, scale and landscaping were reserved from consideration at the outline stage, the outline planning consent establishes the broad parameters within which those matters need to be worked up in detail for the purposes of the Reserved Matters application submissions. It is important to note however that the parameter plans do not absolutely 'fix' matters of detailed design and that deviations from those plans may be accepted under the Reserved Matters applications where the deviations do not substantially alter the original planning permission approved and where the impacts do not result in significant harm to the local area.

### **Reserved Matters - Detailed Design**

11. Core Strategy Policies CP1 (Sustainable Development) and CP3 (General Principles for Development) requires high quality design that respects its context. This requirement is amplified by MDD LP Policies CC03 (Green Infrastructure, Trees and Landscaping) and TB21 (Landscape Character) and the North Wokingham SDL SPD.

### **Layout**

12. The layout has not deviated significantly from the indicative masterplan provided at the time of the outline planning application. As explained within paragraph 5 above, the site area has been reduced, and consequently the total number of dwellings for this parcel has reduced from 93 to 74. This revision excludes the eastern parcel of the site, which is the area surrounding the proposed local neighbourhood centre. Officers consider it is necessary to do this to ensure that the development does not prejudice the centre from coming forward. However, the overall layout remains premised around the delivery of green space to the north and the retention of the most valuable landscaping of the existing site, including trees and hedgerows.
13. The indicative masterplan included with the outline planning permission did show a green openspace running through the centre of the site linking the NDR to the new green space to the north of the site. This green space has been relocated across to the north western boundary of the site within the current layout; adjacent to the retained tree/hedge corridor that will separate Phases 2b and 3. The revisions have a number of benefits, including the removal of a road crossing through the open space, the creation of a unifying green corridor between phases 2b and 3, and the widening of the ecological corridor provided by the retained tree/hedge corridor. Furthermore, the new location of this open space will be pedestrian friendly and provide a clear green vista from the NDR junction with Toutley Road to the green space to the north of the development. Whilst this layout will result in the loss of 2 TPO trees currently situated on the site, suitable replacements are proposed in the newly created green public open space. The approval of the exact details of these replacements will be secured through the detailed landscaping conditions submission.
14. The main access route through the site will be from the approved access off the NDR located at the south western side of the site and runs up through the site to the

public open space in the north of the site. Smaller access roads then radiate from the main route and serve clusters of buildings. Two further secondary accesses will also serve the site, one from further along the NDR to the east, and also from Road 24. Road 24 is being designed and delivered by Bovis, and whilst it is shown on the layout for this reserved matters application, its detailed design does not form part of this application.

15. Buildings front onto the NDR, both sides of the access routes, and also onto the green spaces to the north and western boundaries of the site to provide natural surveillance and discourage anti-social behaviour. Key buildings are also provided at junctions to improve legibility, including the three storey flats which are located on either side of the main road access into the development parcel.
16. Car parking strategies have been provided via the use of both on plot and on street parking provisions. Parking courts have also been provided for the two apartment blocks to accommodate WBC's parking standards. These will be overlooked by the apartments, which again will help to provide natural surveillance.
17. The proposed layout works with the contours of the land and accommodates the drainage requirements of the site.

### **Scale**

18. The scale of development broadly follows the parameter plans that were considered and approved under the outline planning permission.
19. In this regard it is important to note that the parameter plans listed against the outline permission did not absolutely fix the scale of development coming forward via the Reserved Matters application. Scale was one of the matters reserved under the outline application and therefore is to be assessed at this stage where greater detail is submitted. It should be noted that condition 1 (which listed the heights parameter plan) allowed flexibility for this. Agreement in writing is effectively being sought here via this Reserved Matters application.
20. It is also important to note that the outline parameters plan did identify that three-storey development was acceptable in this parcel and specifically on the NDR where an area was shown along the frontage onto the NDR. The NWSDL SPD also identifies that adjacent to the Primary Street (the NDR) buildings should create well enclosed streets and spaces with a more 'urban' feel and that taller properties can be justified where appropriate. Moreover three storey buildings are generally accepted and found along the main distributor roads within the SDL's, as is evident elsewhere within North Wokingham at Kentwood Farm (Mulberry Grove) and also at South Wokingham. Three storey buildings have also been approved along parts of the NDR within Phases 2a and 2b of the wider Matthewsgreen development.
21. The two apartment blocks within Phase 3 are three storeys in height, and similar in scale/bulk and appearance to the apartment blocks approved on the adjacent Phase 2b site. Whilst the position of Block A fronting the NDR is located in an area identified for 2 storeys on the outline heights parameter plan, it is nonetheless considered acceptable in the proposed location. This takes into account its setting in the context of the open space to the north and its proximity to other buildings within the remainder of Phase 3. The second apartment Block B is located in an

area identified for three storeys within the outline application. Wokingham Town Council (WTC) considers that in particular Block B, is of an inappropriate scale and mass. However, the proposed designs of both of the apartment blocks follow that of the approved blocks in Phase 2b. The second floor windows in block B are dormers set within the eaves of the roofslope which serves to reduce the overall perceived height of the building. The two apartment blocks will be located on either side of the main access into the site, and are considered to be appropriately set back from the NDR and away from the other residential dwellings on the site so as not to appear as dominant and overbearing buildings.

22. Some of the houses fronting the NDR are three storeys, which accords with the outline heights parameter plan, although it is noted that the third floor accommodation of these dwellings is achieved by using dormer windows set within the roofslope, which therefore reduces their overall perceived height. Although there are four other three storey properties located further within the site, the second floor accommodation is again designed so that the perceived height of the properties are more like 2 to 2 1/2 storeys. Furthermore, the scale of such properties are not considered to be detrimental to the overall layout and scale of the development or result in wider 'landscape impacts' beyond those that were considered at the time of the outline consent
23. For the reasons set out above, the scale of the buildings within this phase, including the increases in height from those shown on the outline parameter plans are considered acceptable.

### **Appearance**

24. There is a variety of housing typologies proposed including semi-detached and detached houses, and two apartment buildings. The design vernacular follows that used for the adjacent Phase 2b; which is being constructed by the same developer (Linden). The buildings are traditional and incorporate forms and detailing consistent with the local vernacular. In this respect, it pays particular regard to the illustrative street scenes contained within the Design and Access statement that accompanied the outline planning permission.
25. The palette of materials comprises red multi bricks, white and off-white render, vertical tile hanging (including rows of club tiles), red roof tiling, Edwardian and horizontal bar style casement windows, and gable style and lean to porches.
26. The range of housing types and styles ensures that there is diversity in the built form and a range of housing for future occupants. The diversity in terms of the style of the dwellings is achieved through the detailing such as heights, design and roof form. This will help to provide a clear and legible neighbourhood. Wokingham Town Council considers that that the rear elevation of Block B is dominating with a large area of blank wall with no windows. Whilst there is a section of the rear elevation of Block B which has no windows on the first and second floor levels, this area is not considered excessive and does not have a detrimental impact upon the overall design and appearance of the building. As discussed in paragraph 21, the scale of this Block is also considered acceptable. The overall proposed design, scale and appearance is considered good quality and reflects that already approved on Phase 2b.

## Affordable Housing, Dwelling Mix and Standard of Accommodation

27. MDD policy TB05 (Housing Mix) requires that residential development should provide an appropriate density and mix of accommodation reflecting the character of the area. The mix of dwellings proposed is as follows:

### Proposed Housing Mix

	Market	Affordable	Total
1 bed flat	3	-	3
2 bed flat	12	-	12
2 bed house	-	-	-
3 bed house	29	-	29
4 bed house	28	-	28
5 bed house	2	-	2
<b>Totals</b>	<b>74</b>	<b>0</b>	<b>74</b>

28. The affordable housing delivery requirements of the development were established at the time of the outline planning permission. The outline application requires 35% affordable provision across the site as a whole, comprising either 22% provision on site and a commuted sum towards off-site provision (if the assisted living/older person units are not provided) or a 30% on site provision and a lesser commuted sum if the assisted living units are provided. The Council has elected for the assisted living units to be provided, and as such, the on-site affordable housing requirement across the entire site is 30%.

29. The original submission for Phase 3 of the development included the provision of 6 affordable housing units located on the eastern side of the site, next to the local neighbourhood centre. However as advised above this part of the parcel has been reserved for the current time.

30. Once the reserved units for Phase 3 are brought forward, and also taking account of the 60 assisted living affordable units, the remaining on-site requirement to achieve the site-wide 30% affordable housing will be 100 units (or 28%) out of a remaining 352 to be approved through reserved matters submissions. Officers are satisfied that the site wide on-site affordable housing requirement can be satisfactorily achieved and will not be prejudiced through the approval of the Phase 3 proposals.

### Borough Design and Space Standards

31. All of the dwellings exceed the minimum internal floor space requirement as set out in the National Space Standards. The Emmbrook Residents' Association (ERA) consultation response commented upon the living areas provided in some of the dwellings as being inadequate for the bed spaces/number of persons the dwellings are designed to accommodate. However, the National Space Standards only stipulate a minimum total gross internal floor area in relation to the number of proposed bedrooms and bed spaces provided, against which all of the units comply. In addition, the dwellings are required to be constructed to achieve appropriate internal ambient noise levels, which will be assessed under a separate conditions application pursuant to condition 51 of the outline consent.

32. The Borough Design Guide sets a minimum garden depth of 11m. The proposed layout complies with these requirements with all houses meeting or exceeding the

minimum threshold with the exception of 3 units, where the garden depths are marginally less, one being 10m and the other two at 10.9m. It is noted that the ERA commented on the layout and irregular shapes of some of the gardens. However, Officers consider the overall size and usability of these gardens to be acceptable when their overall layout is taken into consideration. It should also be noted that it is difficult to ensure all properties on large scale sites are fully in accordance with standards and in this instance, the units in question are private and therefore prospective buyers have the choice of purchase or not. In addition, permitted development rights of the properties have been removed by conditions 7 and 30 of the outline permission so as to restrict unacceptable encroachment into these important garden spaces.

33. The ERA also raised concern in relation to the external amenity space for the apartment blocks. However, all the upper floor flats would have private balconies which are large enough to accommodate a small table and chairs, which will offer private amenity space to these residents, while the units located at ground level would have a designated area of defensible space. The residents of these units will also benefit from access to the wider open spaces within the development, as well as the nearby SANG. It is therefore considered that the general residential amenities of the occupiers of these units would be acceptable.
34. The Borough Design Guide establishes minimum separation distances of 10m front-to-front across the street, 22m back-to-back and 12m back-to-flank and the proposed layout complies with this guidance. The properties would also each have direct rear garden access.
35. It is therefore considered the development will provide good levels of amenity for future occupants, along with a good mix, balance and quality of dwelling types, tenures and sizes so that a wide range of housing needs can be met. This will ensure that the development is sustainable in meeting the housing needs of the community. The development is therefore considered to accord with the relevant policies of the development plan in this regard.

### **Landscaping**

36. The development will be set within the context of a series of landscaping proposals across the site. There is an existing line of trees and hedgerow running along the north west boundary of the site that separates Phase 2b from Phase 3. This will be retained with the proposals and will form the backdrop to an area of open space which will link up with the open space at the north of the site. This large area to the north of the site is to be landscaped as public open space in accordance with the parameters secured under the outline consent.
37. The landscaped areas within the remainder of the site comprise the planting for the immediate setting of the housing which will include sections of hedge or blocks of shrubs within front gardens to run parallel with the highway/footway. This will help create a sense of enclosure and creating a green corridor along the access roads. Street trees are also proposed at intervals along the access roads, within some rear gardens and within the parking courtyards. This approach is considered acceptable as it is important in terms of not only breaking down the hard landscaped appearance of the roads but also in creating a pleasant residential environment for future residents. These measures, together with the proposed boundary treatments,

will act to soften the built environment and would reduce and mitigate the impact of the development upon the landscape. The landscaping provisions add significant quality to the residential environment.

38. The consultation letter received from a neighbouring resident raised concern in relation to the loss of the two Oak trees on the site as a result of the development. However, two replacement Oak trees are proposed, which will be located within the open space between Phases 2b and 3. Officers consider this to be an acceptable mitigation measure to compensate for the loss of the existing trees. Details of the proposed tree replacements will be secured under the detailed landscaping condition submission.
39. Further consideration of the wider landscaping details will also be included within the detailed landscaping condition submission as required under condition 14 of the outline permission.
40. The open space within the development, including the dry attenuation basin, will be delivered before first occupation of the phase and maintained thereafter for a year by the developer before being transferred (with maintenance contribution) to Wokingham Borough Council. This arrangement is set out within the S106 agreement of the outline planning permission.

#### **Neighbouring Residential Amenity**

41. Core Strategy policy CP3 requires that new development should be of a high quality of design that does not cause detriment to the amenities of adjoining land users. Separation standards for new residential development are set out in section 4.7 of the Borough Design Guide.
42. All dwellings on the site are sufficiently distant from all neighbouring properties, including those approved on Phase 2b and have been designed such that they would not introduce unacceptable relationships of overlooking and loss of outlook, daylight, sunlight or privacy.
43. Noise, disturbance and inconvenience during the construction period can be managed and minimised as far as is reasonable through good practice and through the existing conditions of the outline consent; which require the submission (for the Council's approval) of a Construction Environmental Management Plan and which restrict the hours of construction activity. These matters do not fall for consideration under this Reserved Matters planning application.

#### **Ecology and Biodiversity**

44. Core Strategy Policy CP7, carried forward by MDD LP Policy TB23, requires appropriate protection of species and habitats of conservation value. Design Principle 1b (i-ii) is concerned with protection of ecological habitat and biodiversity features, together with mitigation of any impacts that do arise.
45. The Reserved Matters development follows the principles of ecological protection for this parcel established under the indicative plans and strategies of the outline consent and, subject to the implementation of the various strategies, there would be no significant impact upon ecology. Detailed matters of such will be agreed and

formally secured pursuant to the relevant conditions of the outline consent.

### Transport, Highways and Parking

46. The routes within the site will have a clear hierarchy. The NDR provides the primary street through the wider Matthewsgreen development, from which 5m wide secondary access streets would serve the Phase 3 site. Smaller access roads then radiate from the main internal routes to serve clusters of buildings. All roads are designed in accordance with Manual for Streets.
47. In line with Core Strategy Policy CP6 and MDD DPD Policy CC07, the development will incorporate parking in line with the Council's standards, as follows:

Type	No. spaces	Ratio (74 dwellings)
1. Allocated (excluding garages)	133	1.8
2. Unallocated	23	0.3
3. Visitor	12	0.7
<b>Total Allocated + Unallocated + Visitor</b>	<b>168</b>	<b>2.3</b>
4. Garages	53	
<b>Total Allocated + Unallocated + Visitor + 0.5*Garages</b>	<b>194.5</b>	<b>2.6</b>

48. Allocated parking is proposed on the site by a combination of on-plot parking (drives and garages), formally marked-out parking bays within the public realm areas and communal parking areas serving the flats. On-street parking will also be incorporated as a means of managing the flow of traffic and vehicle speeds. The on-street parking will be managed in accordance with details that will need to be submitted to and approved in writing by the LPA pursuant to condition 29 of the outline consent. The ERA consider that although the proposal meets WBC parking standards, an increase in parking provision for the apartment blocks should be provided. However, the parking standards are based upon census data for tenure/dwelling size and type, and car ownership details. There is therefore no justified basis upon which to require additional parking over and above the Council's adopted Standards.
49. Both the ERA and Wokingham Town Council have commented upon the parking provision in respect of some of the parking spaces between buildings being too small/cramped. However, Highway officers have reviewed the submission, and where such spaces are shown as being less than 3m wide wall to wall, revisions have been requested and provided accordingly. Given that internal garage dimensions are required to be 3m wall to wall, these spaces are in accordance with WBC standards and there would be no justification to require wider dimensions than this externally. The revised layout is now considered to be acceptable in this regard.
50. Where communal parking is proposed for the apartment blocks, these would be overlooked by dwelling units which offer security. These are necessary to accommodate the level of development and parking required within the development and, as such, the communal parking areas are considered acceptable.
51. Refuse bins for the two apartment blocks are located within designated stores within

the car parks. These are located close to the entrance of the car parks, for ease of collection for refuse vehicles. WBC's refuse team no longer favour designated collection points within developments. Tracking information has been provided to demonstrate a refuse vehicle will be able to collect waste from the development in accordance with WBC guidance.

52. Condition 27 of the outline planning consent requires the provision of cycle storage in line with the Council's standards; as currently set out in MDDL Appendix 2. The requirement is for provision of at least one cycle space for dwellings with three or fewer habitable rooms, two spaces for dwellings with four or five habitable rooms and three cycle spaces for larger dwellings. For the houses it is proposed that cycle parking is provided on site within either garages or sheds. Cycle parking for the apartments will be within designated cycle sheds, which are located close to the entrance of the buildings. All dwellings have been provided with sufficient space for cycle storage / parking in accordance with the above requirements.
53. Permeability across the site is good with pedestrian routes linking the development to the surrounding parcels, open space and wider local areas.

### **Flooding and Drainage**

54. Core Strategy Policy CP1 and MDDL Policies CC09 and CC10 establish that new development should avoid increasing and where possible reduce flood risk (from all sources) by managing surface water in a sustainable manner.
55. Surface water run-off will be managed using a Sustainable Drainage System (SuDS) including attenuation in pipelines and attenuation basins, before being discharged at the greenfield run-off rate to the watercourses along the boundaries of the site. Sufficient evidence, including micro drainage calculations, have been provided and demonstrate that the surface water run-off generated up to and including the 1 in 100 year storm event (plus an allowance for climate change) will be accommodated on site so that it will not exceed the run-off from the undeveloped site following the corresponding rainfall event. The development therefore will not exacerbate flood risk over its lifetime. WBC's Flood Risk and Drainage Advisor has reviewed the drainage strategy for the site and is satisfied with the details. The drainage strategy, whilst submitted to support the Reserved Matters Application, will in due course need to be formally agreed under separate application pursuant to the outline conditions.

### **Land Contamination**

56. The site was agricultural in use and low risk in terms of land contamination. Matters of contamination will be considered and mitigated pursuant to the requirements of conditions 25 and 26 of the outline permission and do not need further consideration under this Reserved Matters Planning Application.

### **Sustainable Design and Construction**

57. Core Strategy Policy CP1 requires development to contribute towards the goal of achieving zero carbon development by including on-site renewable energy features and minimising energy and water consumption. MDD DPD policies CC04, CC05 and the Sustainable Design and Construction Supplementary Planning Document

(May 2010) also emphasise this.

58. However, the Government has implemented a new approach for the setting of technical standards for new housing. This has rationalised the pre-existing and many differing standards into a simpler, streamlined system. The Government set out its policy on the application of these standards in decision-taking and plan-making in a written Ministerial Statement. The statement withdrew the Code for Sustainable Homes. As the Code for Sustainable Homes has been withdrawn, WBC can no longer require building to Code Level 4 as a condition to planning consent. However, Part L of Building Regs effectively requires the equivalent energy efficiency as Code Level 4. Given the Government's new approach for the setting of technical standards, Bovis have currently got an application in with the LPA for the removal of condition 44 of the outline permission. Committee therefore need not consider this matter further under this Reserved Matters planning application.

### **Heritage Assets**

59. Policy TB24 of the MDD sets out that the Borough Council will conserve and seek the enhancement of designated heritage assets in the Borough and their settings. There are no designated or undesignated heritage assets within the site. Furthermore, it was established at the outline application stage that the development, subject to the land use distribution and height parameters approved at that time and brought forward here, would not have a detrimental impact upon the special interest or setting of any neighbouring Listed Buildings and Conservation Areas. There is nothing within the current plans that would alter that conclusion.

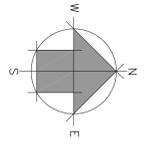
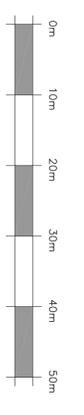
60. In terms of archaeology, results of archaeological trial trenching do not highlight any areas of archaeological potential in relation to the area included within phase 3. WBC Officers therefore accept Berkshire Archaeology's advice that no further archaeological work is required with the Phase 3 area.

### **CONCLUSION**

The reserved matters do not substantially deviate from the principles and parameters established by the outline planning permission. This is a sustainable development that offers substantial public benefit in meeting the housing needs of the community and delivering on Wokingham's development aspirations for the North Wokingham SDL. The application will deliver high quality development in accordance with the Council's spatial strategy and adopted policies and standards and there are no other material planning considerations of significant weight, such as impact upon amenity, appearance etc. that would dictate that the application should be refused. Officers are therefore recommending the application for approval, subject to the conditions listed.

### **CONTACT DETAILS**

<b>Service</b>	<b>Telephone</b>	<b>Email</b>
Development Management and Regulatory Services	0118 974 6428 / 6429	development.control@wokingham.gov.uk



Notes:  
Buildings and boundaries of site are plotted from OS information and have not been surveyed, as such their accuracy cannot be guaranteed.

**Fencing and Boundary Details**  
 - Site boundary line  
 - Proposed screen walls (various heights)  
 - 1.8m high c/h fence.

**Soft Landscaping Details**  
 - Existing trees and hedges to be removed.  
 - Existing trees and hedges during construction all in accordance with BS5837:2012.  
 - Proposed new soft landscaping including trees, indicate only and subject to detailed design and landscape consultation.  
 - 74 total no. units

**Schedule of accommodation:**  
 15no. House Type E 3 bed units  
 2no. House Type G 4 bed units  
 2no. House Type H 4 bed units  
 2no. House Type N 4 bed units  
 2no. House Type S 3 bed units  
 1no. House Type S 3 bed unit  
 17no. 2 bed apartments  
 74 total no. units

REV	DATE	AMEN APP REVISION
F	16/03/17	01
E	20/03/17	02
D	22/03/17	03
C	01/04/17	04
B	06/04/17	05
A	28/02/17	06

**Architectus**  
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 TEL: 01293 575171 FAX: 01293 575171 EMAIL: info@architectus.co.uk

CLIENT: Linden Homes Chiltern  
 PROJECT: Phase 3, Matthewsgreen,  
 Wokingham, Berkshire.

DRAWING: Planning Site Layout and  
 Boundary Treatment

DRG. NO. 16/033/011F  
 SCALE: 1:500@A1  
 DATE: Feb 2017  
 DRAWN: CW  
 APPROVED: JR

This drawing is the copyright of Architectus Limited. The contractor should check all dimensions on site. No compliance with the current requirements of statutory regulations (Building Regulations, British Standards etc.)

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## Wokingham Town Council

### Comments on Application 170618 – Matthewsgreen Phase 3

#### **GENERAL LAYOUT**

We welcome the provision of footways adjacent to most of the roadways in the development.

The general layout of the development would appear to have one major problem. The three storey blocks of apartments and (in particular Block B) are of an inappropriate scale and mass compared to the surrounding buildings. The roof space gives the appearance of at least a 4 storey building making the apartments overpowering and out of proportion (CP03). The buildings should be reduced in height by incorporating the third storey in to the roof.

The rear elevation of Block B is also immensely dominating having a huge area of blank wall with no windows.

#### **AFFORDABLE HOUSING**

This development does not provide the required level of affordable housing at only 6 units out of a total of 93. It relies on the shortfall being made up in further phases of the project which as yet are unapproved.

#### **PARKING**

A number of units have cramped parking spaces between the flank walls of the properties. These are believed to be unsuitable for general use by residents due to their size and difficult access. This we believe will lead to on street parking which will cause major issues due to the 5m width of the roadways. Simple mathematics supports this. Two 2.5m wide spaces in an area of 6m in width between two walls. This assumes the buffer area between the spaces and the walls is 1m in total.

The assumed parking parameters are:-

Gap between parked vehicles and the flank walls 0.6m

Width of an average family car (Ford Focus including wing mirrors 2.01m)

Space required to open a door with reasonable access 0.85m

Two vehicles parked as above require 5.2m leaving just 80cm to open one door on each vehicle. This assumes that:

both vehicles are parked within 0.6m of the flank walls

no vehicle wider than 2m is owned by the resident, and

everyone parks within the specified distance of the flank walls

Driving forward into one space will require a space of at least a car width and door opening for the driver to exit, requiring a width of close to 3.45m in a space 3m wide. This is calculated as the sum of car width (2m) plus gap between vehicle and the flank walls (0.6m) plus space required to open a door on one side of the vehicle (0.85m). The parking space width being calculated as the sum of bay width (2.5m) plus buffer around the spaces (0.5m) giving an effective space of 3m. Driver and passengers leaving by opposite sides of the vehicle will be impossible.

We believe such an arrangement to be impractical.

In addition to the above:

access to many of the parking spaces in the development assumes nothing else is parked in the area required to manoeuvre (particularly where parallel parking is employed)

the central area of the site appears to have little visitor/unallocated parking

## **ROAD WIDTHS**

The road widths within the development are stated as being 5m.

Section 4.2.1 of the Transport Statement says that 'a delivery vehicle can safely pass a large car on the main access roads'. This has been shown by a swept path analysis which shows that provided NOTHING else is parked on the roadside or bends access is just about possible.

It assumes a delivery vehicle width of 2.5m and a large family car width of 1.8m. We regret that this analysis would appear to be flawed. It uses vehicle wheelbase dimensions which do not take account of items such as wing mirrors. A Ford Focus is not a large family car but with wing mirrors included it is over 2m wide. A delivery vehicle including wing mirrors is around 2.8m wide. This will give a passing clearance of less than 20 cm. On bends and other areas of the site this clearance would turn into a negative number.

A large family vehicle such as a Ford S-MAX is 2.12m wide when mirrors are included, giving this a passing distance of just 8cm.

We do not believe 20cm (let alone 8cm) clearance provides a safe passing distance between vehicles and allows them to pass 'in relative comfort' as stated in the section.

Section 4.2.4 of the Transport statement states an intention in the 4.1m wide shared surface areas of ensuring 'pedestrians need not feel intimidated by vehicles'. We do not believe this is possible in any shared surface areas.

Section 4.2.6 of the Transport Statement states that the visibility splays are in accordance with guidelines for vehicles travelling at no more than 20mph. In order to achieve compliance how will this speed limit be enforced?

## **CONCLUSION**

Whilst we welcome many of the features of this development we do not support it in its current form for the reasons stated above.

Planning & Transportation Committee

5<sup>th</sup> April 2017

# Agenda Item 19.

Development Management Ref No	No weeks on day of committee	Parish	Ward	Listed by:
170686	Planning Performance Agreement	Barkham, Finchampstead, Swallowfield, Arborfield	Barkham, Finchampstead South, Swallowfield, Arborfield	N/A SDL Major Development

**Applicant** Redrow Homes Limited C/O Pegasus Group  
**Location** Arborfield Garrison and adjoining land **Postcode** RG2 9LN  
**Proposal** Reserved Matters application pursuant to Outline Planning Consent O/2014/2280 for the erection of 179 dwellings with access from the Nine Mile Ride Extension (NMRE), with associated internal access roads, paths, circulation areas, car parking including garages, landscaping, open space and associated infrastructure and works. (Parcels H, I and J)  
**Type** Reserved Matters  
**PS Category** 7  
**Officer** Alex Thwaites

**FOR CONSIDERATION BY** Planning Committee on 12/07/17  
**REPORT PREPARED BY** Head of Development Management and Regulatory Services

## SUMMARY

This application relates to land within the 'Southern Gateway' character area of the Outline planning consent granted under planning application O/2014/2280 which established the principle of access to the site together with development parameters. The Parcels (H-J) are located to the east of the A327, south of the Nine Mile Ride Extension (approved under 153336 and open) and west of Sheerlands Road. The development area is opposite the third phase of development on the former Garrison site (Parcels A-G) and there are no existing neighbouring properties within close proximity.

The principle of development in this location has been established through its allocation by policy CP18 of the Core Strategy and through the Spatial Framework Plan within the Arborfield Garrison Supplementary Planning Document together with the outline planning permission.

The overarching vision of the SDL is to provide a co-ordinated approach to the delivery of infrastructure and services ensuring that developments are of a high quality and are sustainable. This includes the provision of schools, community facilities, good quality open space and appropriate local transport and links. The outline application for Arborfield Garrison was supported by an Infrastructure Delivery Plan (IDP) which established how the necessary infrastructure could be delivered. A S106 legal agreement secured the proportion of infrastructure attributable to the development at Arborfield Garrison and triggers for its delivery.

This current application is for reserved matters application for 179 dwellings, which represents phase four of the Arborfield Garrison development. The application seeks permission for the details of appearance, landscaping, layout and scale only. Conditions

applications have also been submitted in parallel to the reserved matters which seek approval for other detailed matters such as flooding and drainage.

The application is before the Planning Committee as it relates to major development that is recommended for approval. It is considered that the development would be a sustainable development that represents the fourth stage of the Arborfield Garrison development and would not have a significant detrimental impact on the character of the area or on existing residents.

The site levels vary within the parcels, however, as a broad overview the site gently slopes upwards towards the A327. The development site is split into sections by existing vegetation classified as important hedgerows and by a pipeline which dissects the site. The masterplan has been designed in order to retain this feature and incorporate it into an area of open space which also includes cycle and pedestrian links between the parcels. The Sustainable Drainage Systems (SUDS) have been designed as part of the site layout and there is sufficient capacity within the site to accommodate the development.

The development is intrinsically linked with the Nine Mile Ride Extension (NMRE) which runs along the north boundary of the development parcel and prescribes the access into the site in three locations. The NMRE includes its own SUDs features, linear park and bridleway that all link into Parcels H-J as well as the wider Strategic Development Location.

The proposal would provide for an adequate level of amenity for the future occupants of the dwellings together with an acceptable impact on ecology, traffic, highway safety and flood risk in addition to contributing to the delivery of infrastructure provided jointly by the wider SDL.

In design terms, the proposal meets all the council's standards, in particular internal space, garden depths and parking standards. Equally, the proposals broadly follow the parameters set out in the outline application and the development comprises of generally lower density residential housing, primarily two storeys in height with some three storey buildings in key locations. The design of the units which appear more traditional in appearance will introduce a new character area to the SDL development as well as being compliant with Wokingham Borough Council policies.

The proposals are considered to be acceptable and therefore it is recommended that Reserved Matters are approved subject to conditions outlined below.

#### **PLANNING STATUS**

- Strategic Development Location (SDL)
- Modest Development Location
- Countryside
- Thames Basin Heaths Special Protection Area 5km – 7km Zone
- Site of Special Scientific Interest 500m Buffer - Longmoor Bog
- Affordable Housing Thresholds
- Farnborough Aerodrome Safeguarding Consultation Zone
- Potentially Contaminated Land - Sewage Works (Disused)
- TPOs Served
- Bat Roost

## RECOMMENDATION

**APPROVE RESERVED MATTERS** subject to the following conditions:

### 1. Outline Application Compliance

Nothing herein contained shall be deemed to affect or vary the conditions imposed by planning permission O/2014/2280 dated 02/04/2015 which conditions shall remain in full force and effect save in so far as they are expressly affected or varied by this permission.

### 2. Plans

Approved plans – details to be submitted as part of members update (awaiting revised / updated plans)

*Reason: For the avoidance of doubt and to ensure that the development is carried out in accordance with the application form and associated details hereby approved.*

### 3. Landscape Maintenance and Management

Prior to the first occupation of the development a landscape maintenance and management plan, including long term design objectives, management responsibilities, timescales and maintenance schedules for all landscape areas, other than privately owned, domestic gardens, shall be submitted to and approved in writing by the local planning authority. The landscape maintenance and management plan shall be carried out as approved unless otherwise agreed in writing by the Local Planning Authority.

*Reason: In order to ensure that provision is made to allow continuing enhancement and maintenance and management of the landscaping hereby approved. Relevant policy: Core Strategy policy CP3 and Managing Development Delivery Local Plan policies CC03 and TB21.*

### 4. Highway Construction Details

Prior to the commencement of development, full details of the construction of roads and footways, including levels, widths, construction materials, colour palette, service margin materials, depths of construction, surface water drainage and lighting shall be submitted to and approved in writing by the local planning authority. The roads and footways shall be constructed in accordance with the approved details to road base level before the development is occupied and the final wearing course will be provided within 3 months of occupation, unless otherwise agreed in writing by the local planning authority.

*Reason: To ensure that roads and footpaths are constructed to a standard that would be suitable for adoption as publicly maintainable highway, in the interests of providing a functional, accessible and safe development. Relevant policy: Core Strategy policies CP3 & CP6.*

### 5. Surfacing of Access

No building shall be occupied until the vehicular access has been surfaced with a permeable and bonded material across the entire width of the access for a distance of 10 metres measured from the carriageway edge.

*Reason: To avoid spillage of loose material onto the highway, in the interests of road safety. Relevant policy: Core Strategy policy CP6.*

### 6. Access Prior to Occupation

No building shall be occupied until the access has been constructed in accordance with the approved plans.

*Reason: In the interests of highway safety and convenience. Relevant policy: Core Strategy policies CP3 & CP6.*

### **Informatives**

1. Nothing herein contained shall be deemed to affect or vary the conditions imposed by planning permission O/2014/2280 dated 02/04/2015 which conditions shall remain in full force and effect save in so far as they are expressly affected or varied by this permission. Equally, this permission should be read in conjunction with the legal agreement under section 106 of the Town and Country Planning Act that accompanies planning permission O/2014/2280 dated 02/04/2015.
2. The applicant is advised that the Council seeks that employers or developers within the borough commit to using local labour / contractors where possible. This should include:
  - Advertisement of jobs within local recruitment agencies / job centres;
  - Recruitment and training of residents from the local area;
  - Seek tender of local suppliers or contractors for work.
3. **Work on Highway**  
The Corporate Head of Environment at the Council Offices, Shute End, Wokingham should be contacted for the approval of the access construction details before any work is carried out within the highway. This planning permission does NOT authorise the construction of such an access.
4. **Mud on Road**  
Adequate precautions shall be taken during the construction period to prevent the deposit of mud and similar debris on adjacent highways. For further information contact Corporate Head of Environment on tel: 0118 974 6302.
5. **Highway Management**  
Any works/events carried out either by, or at the behest of, the developer, whether they are located on, or affecting a prospectively maintainable highway, as defined under Section 87 of the New Roads and Street Works Act 1991, or on or affecting the public highway, shall be coordinated under the requirements of the New Roads and Street Works Act 1991 and the Traffic management Act 2004 and licensed accordingly in order to secure the expeditious movement of traffic by minimising disruption to users of the highway network in Wokingham.
6. **Utilities**  
Any such works or events commissioned by the developer and particularly those involving the connection of any utility to the site, shall be co-ordinated by them in liaison with Wokingham Borough Council's Street Works Team, (telephone 01189 746302). This must take place at least three month in advance of the works and particularly to ensure that statutory undertaker connections/supplies to the site are coordinated to take place wherever possible at the same time.
7. **Noise**  
The attention of the applicant is drawn to the requirements of Section 60 of the Control of Pollution Act 1974 in respect of the minimisation of noise on construction and demolition sites. Application, under Section 61 of the Act, for prior consent to the works, can be made to the Environmental Health and Licensing Manager.

<b>PLANNING HISTORY</b>		
SO/2010/0611	EIA scoping opinion	23/4/2010
O/2013/0600	Outline application for 2000 dwellings and supporting infrastructure.	Withdrawn 19/11/2013
O/2014/2280	<p>OUTLINE PERMISSION FOR: Demolition of buildings and phased redevelopment of Arborfield Garrison and adjoining land for: Up to 2,000 new dwellings (including up to 80 units of extra care housing). District centre comprising a foodstore up to 4,000 sqm gross with up to a further 3,500 sqm (gross) floor space within Classes A1, A2, A3, A4, A5, B1, D1 and D2 (with residential above - Class C3)), and transport interchange, village square, car parking, servicing and drop off area. Up to a further 1,500 sqm (gross) floor space within Classes D1 and D2. Neighbourhood centre to provide up to 300 sqm (gross) floor space within Classes A1, A2, A3, A4, A5, B1, D1 and D2, with parking/servicing area. Secondary school for up to 1,500 pupils (Class D1) including sports pitches, flood-lit all-weather pitch, and indoor swimming pool and parking areas. Up to three-form primary school (Class D1) with sports pitch and parking areas. Associated phased provision of: car parking; public open space including sports pitches, informal/incidental open space, children's play areas including multi-use games area (MUGA), skate park, community gardens/allotments; landscaping/buffer areas; boundary treatments; new roads, footpaths, cycleways and bridleways; sustainable urban drainage systems, including flood alleviation works.</p> <p>PART 2 - FULL PERMISSION FOR phased development of: Creation of two new areas of Suitable Alternative Natural Greenspace (SANGS) (In the north-eastern part of the application site ("Northern SANGS") and at West Court ("West Court SANGS") including car parking areas, path/walkways, fencing and associated landscaping; re-use of existing MoD gymnasium for sports/community uses/centre (Classes D1/D2; new roundabout junction to A327 Reading Road; junction improvements to Langley Common Road, Baird Road and Biggs Lane; junction improvements and new access at Biggs Lane/Princess Marina Drive;</p>	Approved 02/04/2015

	re-use and improvements to existing site accesses from Biggs Lane.	
150162	Reserved Matters application for the erection of 113 dwellings with access from Biggs Lane and Princess Marina Drive, with associated internal access road, parking, landscaping and open space, footpaths and sustainable Urban Drainage (Suds) – ‘Phase One’	Approved 26/11/2015
153336	Reserved Matters application pursuant to planning consent O/2014/2280. The application relates to the Nine Mile Ride Extension, School Access Road and A327 Roundabout. Details of access, appearance, landscaping, layout and scale to be considered.	Approved 24/03/2016
161536	Application for Reserved Matters for the erection of 127 dwellings together with access from Princess Marina Drive with associated internal access roads, landscaping, open space, footpaths and sustainable urban drainage (SUDS), relating to (parcel T) land – ‘Phase Two’	Approved 14/09/2016
161747	Application for Reserved Matters for the erection of 223 dwellings together with access from Sheerlands Road and the Nine Mile Ride Extension, with associated internal access roads, parking, landscaping, open space, footpaths, bridleways and sustainable urban drainage (SUDS), relating to Parcel A-G land – ‘Phase Three’	Approved 23/11/2016
171333	Application for approval of Reserved Matters pursuant to Outline Planning Consent O/2014/2280 for 79 dwellings with access from Biggs Lane, with associated internal access roads, parking, landscaping and open space, footpaths/cycleways, Sustainable Urban Drainage (SuDs) sub-station and gas governor (Parcel U2) – ‘Phase Five’	Pending Decision

### SUMMARY INFORMATION

#### For Residential

Site Area	6.48 ha
Existing units	MOD Use
Proposed units	179
Proposed density - dwellings/hectare	35 d/ha
Number of affordable units proposed	36 (20%) and 15% commuted sum
Previous land use	Brownfield site (MOD)
Proposed parking spaces	375 spaces (491 including garage spaces)

## CONSULTATION RESPONSES

Berks, Bucks and Oxon Wildlife Trust	No objection subject to the proposal being carried out in accordance with the submitted documents and mitigation [Officer Note: Condition 2 secures all documents and plans and the proposal must be carried out in accordance with said documents/plans]
Berkshire Archaeology	No objection.
WBC Biodiversity	No comments received at time of writing.
WBC Building Control	No comments received.
WBC Children Services	No comments received.
WBC Community Infrastructure	No comments received.
WBC Conservation Officer	No objection.
Crime Prevention Design Officer	No comments received.
WBC Drainage	No objection.
WBC Economic Sustainability Team	No objection.
WBC Education Services	No comments received.
Environment Agency	No comment.
WBC Environmental Health	No objection.
WBC Health and Wellbeing	No comments received.
WBC Highways	No objection – subject to conditions
Highways England	No objection.
Historic England	No comment.
WBC Trees & Landscape	No objection – subject to conditions
WBC Libraries	No comments received.
Natural England	No comment.
Network Rail	No comments received.
WBC LUTT (Policy)	No objection
WBC Public Rights of Way	No comments received.
Royal Berkshire Fire and Rescue	No objection – subject to provision of public mains for water supply to effectively fight fire. [Officer note: these are now covered until Building Regulations and are required to be provided]
South East Water	No comments received.
South West Train	No comments received.
Southern Gas Networks	No comments regarding this particular application, however reference to the previously approved outline – no objection.
Sport England	No comments received.
SSE Power Distribution	No comments received.
Thames Water	No objection.
WBC Waste Services	No objection.

## REPRESENTATIONS

<b>Arborfield Parish</b>	'The Parish Council has no objections to this application'
<b>Finchampstead Parish</b>	No objection.

<b>Barkham Parish</b>	No comment.
<b>Local Members</b>	No comments received.
<b>Neighbours</b>	No neighbour representation received
<b>PLANNING POLICY</b>	
<p><i>National Policy</i></p> <p>National Planning Policy Framework 2012  Technical Guidance to the National Planning Policy Framework 2012</p> <p><i>South East Plan 2009</i></p> <p>Saved policy NRM6 - Thames Basin Heaths Special Protection Area</p> <p><i>Wokingham Borough Core Strategy policies:</i></p> <p>CP1 - Sustainable Development  CP2 - Inclusive Communities  CP3 - General Development Principles  CP4 - Infrastructure Requirements  CP5 - Housing Mix, Density and Affordability  CP6 - Managing Travel Demand  CP7 - Biodiversity  CP8 - Thames Basin Heaths Special Protection Area  CP9 - Scale and Location of Development Proposals  CP10 - Improvements to the Strategic Transport Network  CP11 - Proposals outside Development Limits (including countryside)  CP13 – Town Centres and Shopping  CP17 - Housing delivery  CP18 – Arborfield Garrison Strategic Development Location</p> <p>Appendix 7 – Additional Guidance for the Development of Strategic Development Locations</p> <p><i>Managing Development Delivery Document (Local Plan) (adopted February 2014)</i></p> <p>CC01 – Presumption in Favour of Sustainable Development  CC02 – Development Limits  CC03 - Green Infrastructure, Trees and Landscaping  CC04 - Sustainable Design and Construction  CC05 - Renewable energy and decentralised energy networks  CC06 - Noise  CC07 - Parking  CC08 - Safeguarding alignments of the Strategic Transport Network &amp; Road Infrastructure  CC09 - Development and Flood Risk (from all sources)  CC10 - Sustainable Drainage  TB05 - Housing Mix  TB07 – Internal Space Standards  TB08 - Open Space, sport and recreational facilities standards for residential development  TB09 – Residential accommodation for vulnerable groups</p>	

TB12 – Employment Skills Plan  
 TB15 – Major Town, and Small Town/District Centre development  
 TB16 – Development for Town Centre Uses  
 TB20 – Service Arrangements and Deliveries for Employment and Retail Use  
 TB21 - Landscape Character  
 TB23 - Biodiversity and Development  
 TB24 – Designated Heritage Assets (Listed Buildings, Historic Parks and Gardens, Scheduled Ancient Monuments and Conservation Areas)  
 TB25 - Archaeology  
 TB26 – Buildings of Traditional Local Character and Areas of Special Character  
 SAL05 - Delivery of Avoidance Measures for the Thames Basin Heath Special Protection Areas

### **Supplementary Planning Documents**

Arborfield Garrison Strategic Development Location Supplementary Planning Document (October 2011)  
 Infrastructure Delivery and Contributions Supplementary Planning Document (October 2011)  
 Wokingham Borough Design Supplementary Planning Document (2012)  
 Wokingham Borough Affordable Housing Supplementary Planning Document (July 2013)  
 Sustainable Design and Construction Supplementary Planning Document (28 May 2010)  
 Planning Advice Note, Infrastructure Impact Mitigation, Contributions for New Development (Revised 2014)  
 Barkham Village Design Statement (re-adopted as an appendix to the Borough Design Guide SPD in May 2012)  
 Emerging Arborfield and Newland Village Design Statement (post-consultation, now being considered for adoption)

The Council's parking standards as set out within appendix 2 of the Managing Development Delivery Local Plan and referred to in para 4.38 of the Core Strategy

## **PLANNING ISSUES**

### **Principle of Development and Infrastructure Delivery**

1. The application site forms part of a larger area designated under the Wokingham Borough Core Strategy as the Arborfield Garrison Strategic Development Location (SDL).
2. Wokingham Borough Core Strategy policy CP17 establishes a requirement to provide at least 13,487 new dwellings with associated development and infrastructure in the period 2006-2026. The majority of this new residential development will be in four SDLs, of which Arborfield Garrison is one of these. Policy CP18 identifies that the Arborfield Garrison SDL will deliver a sustainable, well designed mixed use development of around 3,500 dwellings and associated infrastructure.
3. Core Strategy Policy CP18 is amplified by Appendix 7 of the Core Strategy, the Arborfield Garrison Strategic Development Location Supplementary Planning Document (SPD) and Infrastructure Delivery and Contributions SPD, which address the associated infrastructure impacts across the whole borough. These documents establish a requirement for a sustainable, well designed, mixed use development

and make clear that a co-ordinated approach to the development of the SDL will be required to deliver the necessary infrastructure, facilities and services to meet the needs of the expanded community.

4. Outline planning permission for the site was granted by Wokingham Borough Council on 02/04/2015. This established the principle for development for the site together with access for up to 2,000 new dwellings, district centre, neighbourhood centre, secondary school, primary school and two Suitable Alternative Natural Greenspace (SANGS). These were considered against the relevant Core Strategy policies and Local Plan policies. The current application seeks reserved matters approval for appearance, layout, landscaping and scale. Other issues such as updating the Flood Risk Assessment and Phasing are being considered under separate conditions applications.
5. The outline consent included an Infrastructure Delivery Plan and S106 legal agreement. The legal agreement secures the coordinated delivery of the infrastructure necessary to support the development and fair share of the SDL wide infrastructure. These included contributions towards off-site infrastructure and services such as roads, education, sports facilities, community facilities and green infrastructure. In addition, it secured an affordable housing contribution in accordance with the Core Strategy and Infrastructure Delivery and Contributions Supplementary Planning Document.
6. The details approved under the outline planning consent established parameters for the development including general site layout including the quantum of development, indicative location of housing, open space, density and general heights of buildings. Access to the site was also established.
7. The proposal represents the fourth phase of residential development on the Arborfield Garrison. Previous approved proposals include Parcel 'O1', 'T', 'A-G' the Nine Mile Ride Extension (NMRE) and the Arborfield Secondary School.

#### **Site Description**

8. The application site relates to an area of land within the Outline application that lies in the character area of the 'Southern Gateway'. The application site, referred to as 'Parcels H-J' at outline, is immediately adjacent to the A327, the Nine Mile Ride Extension (NMRE) and Sheerlands Road.
9. As described in the Outline application the existing land use of the site comprises of the vacated Arborfield Garrison (MOD use), which is currently not accessible to the public.

#### **Dwelling Mix:**

10. MDD LP Policy TB05 requires an appropriate housing mix, which reflects a balance between the character of the area and the current and projected needs of households.
11. The accommodation on the site would be provided within both apartments and houses. The mix of dwelling types provides a range of house types, smaller units, 2 bed dwellings, with larger houses and apartment buildings, which allows for an appropriate housing mix, which reflects a balance between the character of the area and the current and projected needs of households on the market.

12. The table below shows a breakdown of the mix of dwelling types and units on the first phase.

		Private	Affordable	Total
Apts	1 bed	0	8	8
	2 bed	27	9	36
Houses	2 bed	3	10	13
	3 bed	68	8	76
	4 bed	37	1	38
	5 bed	8	0	8
	Total	143	36	179

13. The proposal is considered acceptable and provides a good balance of accommodation on the site.

### Masterplan – Layout

14. Core Strategy Policies CP1, *Sustainable Development* and CP3, *General Principles for Development* requires high quality design that respects its context. This requirement is amplified by MDD LP Policies CC03, *Green Infrastructure, Trees and Landscaping* and TB21, *Landscape Character* and Arborfield Garrison SPD which requires development proposals to protect and enhance the Borough’s Green Infrastructure, retaining existing trees, hedges and other landscape features and incorporating high quality - ideally native – planting as an integral part of any scheme, within the context of the Council’s Landscape Character Assessment.

15. Core Strategy policy CP18, *Arborfield Garrison Strategic Development Location* sets out the concept rational for the design parameters for the Arborfield Garrison SDL and these are outlined in further detail in Appendix 7 of the Core Strategy. This states that:

*The attractive rural setting, which requires a design response to ensure the development, is absorbed into the landscape, taking account of natural features including watercourses, and to ensure a sense of the landscape permeates the development through an open space strategy*

16. Further design guidance is provided by the Arborfield Garrison SPD in section 4. Design principle 3 is concerned with character and states that there should be diversity and distinction in the SDL, which should be enhanced through the application of character typologies.

17. It is important to set out the site in the context of the wider planning permission for Arborfield Garrison. The application parcel itself forms a part of the Arborfield Garrison planning unit. In order to ensure consistency of design for the entire development, the Arborfield Garrison planning consent established design principles including indicative parameter plans for development limits, density and building heights. Further design guidance was also included within the Design and Access Statement.

18. To build upon these principles, a requirement of the outline planning permission was for the applicant to submit a design code with the submission of reserved

matters applications. The purpose of the design code is to ensure continuity of development across the site and the application of Garden Village principles in the design. In accordance with this requirement, the applicant has provided these details which have demonstrated the overarching principles for the entire site and are acceptable. The design principles outlined in the code will also be expected to be incorporated to the wider Arborfield Garrison planning unit.

19. The submitted masterplan retains the principles within the outline planning application and design code. To reflect this and inform diversity of development, the applicant has ensured differing street typologies, building heights and landscaping have been applied to promote variety and also manage vehicle speeds throughout the site. In addition, consideration has been applied to the relationship of the site and the constraints of the surrounding area through the application of these differing character areas.
20. The layout of the wider Arborfield Garrison development has been determined through the previous outline (O/2014/2280) and this included access into the development parcel. The parcels have been designed to decrease the street hierarchy off these main accesses to promote a variety of street typologies and promote the differences between the character areas. The layout of this parcel has been designed in accordance with site constraints, namely the pipeline and existing landscaping, WBC standards and good practice and as such is considered acceptable.

#### **Masterplan – Density**

21. Design principles 3 (a-g) are outlined in the Arborfield Garrison SPD and are concerned with the character of the development, including the density of development. The Core Strategy approach for the Arborfield Garrison SDL in respect to density is outlined in Appendix 7 which indicates an average density of 30-35 dwellings per hectare (d/ha) across the SDL. Additionally the Arborfield Garrison outline planning consent established 'Parcels H, I and J' to be of 'medium density' and therefore prescribed as 27-37 dwellings per hectare (d/ha).
22. The overall density proposed by the application equates to 35 d/ha which will include a provision of 20% on-site affordable housing. The remaining 15% is to be commuted off-site. There is no particular area within the development parcel that boasts a significantly higher density compared with the rest of the site, the units are distributed relatively evenly across the parcels with the majority of units being two storeys in height with the occasional 3 storey apartment building being utilised at focal points within the development.
23. This approach is considered consistent with the advice provided by the SPD, the previously approved outline application and the overall density of development is in line with Appendix 7 of the Core Strategy and is therefore acceptable.

#### **Masterplan – Design and Appearance**

24. In terms of the detailed design of the buildings, the Arborfield Garrison SPD recommends that the existing built form should be used to inform the design. The Design and Access Statement and Design Code include a character analysis of the surrounding area and picks out themes that are common in the locality. These have been used to help inform the design style of the dwellings which take clear character references from the local vernacular architecture. The design should also

be influenced and consistent with the Garden Village Principles as well as relating well with previously approved parcels.

25. Throughout the site, the applicant has provided a good range of housing types and styles ensuring that there is diversity in the built form and a range of housing for future occupants. The application of character areas outlined above also helps provide variety throughout the site. In addition, there is diversity in the style and heights of the dwellings achieved through detailing such as rendering on some house types and brick work details on others. This helps to provide a clear and legible neighbourhood.
26. The development parcel has two natural segments due to the provision of a central green strip that includes the attenuation basin, pipeline and an existing hedgerow, as per the parameters at outline stage. While these sections appear physically separate they provide a pedestrian / cycle access and can be used as emergency access to connect the two segments and connect to the wider Arborfield network. Public spaces are overlooked by dwellings to ensure surveillance and security. The designs of the dwellings and outline parameter plans, allow for three storey building to create 'landmark buildings' at strategic locations as well as fronting onto the northern section of the Nine Mile Ride Extension. Equally the materials used and details, such as render and timber cladding, is not only reflective of the surrounding properties but also reflective of the first phases of development in the Arborfield Garrison development location.
27. Whilst the proposal introduces a more traditional design into the SDL, the NPPF states *planning policies and decisions should not attempt to impose architectural styles or particular tastes and they should not stifle innovation, originality or initiative through unsubstantiated requirements to conform to certain development forms or styles. It is, however, proper to seek to promote or reinforce local distinctiveness.* The proposal consists of features that are found in the local vernacular. It is considered that the applicant has carefully considered the design in the context of the surrounding area and proposes a design that is respectful to the character of the area.
28. The overall design approach for all of the dwellings is considered appropriate, although they differs from that of the consented schemes on the SDL in their design, offering a new distinct character in this area and is generally considered successful in its approach and respects the context of the site location and in accordance with the advice provided by the NPPF, policies CP1 and CP3 together with the Arborfield Garrison SPD and Supplementary Planning Guidance. The proposals are therefore considered acceptable.

#### **Masterplan – Landscape**

29. Much of the development site is open fields at present whilst there is not a huge presence of significant existing vegetation; the masterplan has carefully considered the landscape and incorporated this into the design. The development parcels are enclosed by existing mature vegetation and these are to be retained and reinforced in areas where required.
30. Elsewhere in the Parcels there are existing and proposed hedgerows that break up the development. Again the layout has been carefully considered to ensure that this would be retained with appropriate buffers from the built form. Although street

planting is limited in some of the internal streets of the site, some key tree planting has been achieved with views through to green spaces and landscaping. This helps soften the development and there is also hedgerow planting to the front of the dwellings in front garden areas where space is limited for tree planting.

31. Within the development, landscaping is proposed throughout the residential areas. Landscaping strips and SUDs have been established through the primary infrastructure and these will include tree planting. Along the main roads in the development a regular line of trees is proposed which, when established, will give this area an avenue type feel and also help distinguish it as part of the primary route for the site. More informal planting is proposed in the other character areas which include trees and low level planting. These measures together with the proposed boundary treatments would reduce and mitigate the proposed development's impact on the landscape and is generally consistent with the advice provided by the Arborfield Garrison SPD.
32. Officers consider that the applicant has carefully considered the landscaping within the site masterplan. Where possible existing trees are to be retained and there would be no loss of any species considered significant by the Landscape Officer. Semi mature trees are proposed within the site layout which would help soften the built form and provide a verdant character along the areas of public open space. Where existing vegetation has been lost, this has been established at the outline planning stage and is mitigated against with new planting. Particular attention has been given to the northern boundary of development parcels and how the proposal links with the Nine Mile Ride and Sheerlands Road.

#### **Masterplan – Conclusion**

33. The design approach taken to inform the layout and appearance of the buildings is considered to respond well to the existing landscape features, context of the site and surrounding area. The applicant has satisfactorily demonstrated a comprehensive approach to the delivery of the dwellings under consideration by this reserved matters application. This has been achieved through applying the principles of the design code in the proposed layout and the principles of the Garden Village.
34. Overall, it is considered that the proposal would not have a significant detrimental impact on the character of the area and is broadly in accordance with the principles of the relevant planning policies and supplementary planning advice and represents high quality development.

#### **Residential Amenity – Impact upon Neighbouring Properties**

35. Core Strategy policy CP3 requires that new development should be of a high quality of design that does not cause detriment to the amenities of adjoining land users. Separation standards for new residential development are set out in section 4.7 of the Borough Design Guide.
36. The proposal is subject to parameters set at outline stage that includes both separation distances and open space plans. The development parcel is located in the most southwestern section of the Arborfield Garrison development away from neighbouring properties. It is therefore considered that there would be no significant impacts to existing residents in terms of overlooking, loss of light and overbearing.

### **Residential Amenity – Noise**

37. Whilst it is acknowledged that there would be a greater level of activity in the area arising from the delivery and future occupation of the housing, this would not be to an extent that is unusual in a residential area. As the site has been identified as being suitable for housing, the principle of residential development in this area has been established. The outline permission also secured via condition and s106 the construction routes for the proposal in order to keep disruption to a minimal. Construction activities would be temporary and Condition 68 of the Outline Planning consent controls the hours of operations to 0800-1800 on Mondays to Fridays and 0800-1300 on Saturdays.
38. It is considered that the proposal would not result in any significant harm to the amenity of the existing residents and is in accordance with policies CP1 and CP3 and supplementary planning guidance.

### **Residential Amenity – The Amenity of Future Occupants of the Development**

39. The Borough Design Guide and MDD LP policy TB07 set out minimum thresholds for private amenity space and internal space standards which new residential development should be assessed against.

#### *Private Amenity Space*

40. The Borough Design Guide sets a minimum garden depth of 11.0 metres. In terms of the submitted masterplan, the proposed layout complies with these requirements with all houses meeting the minimum threshold, apart from eight of the 179 units. However, the majority of these are just below 11 metres; the units front either onto or are in close proximity to open space within the layout. Equally, for some of the proposed houses, the garden areas are wide which increases the area of amenity space. Overall, it is considered the development will provide acceptable levels of private amenity space for the future occupants of the dwellings and is in accordance with policy.

#### *Internal Space Standards*

41. With respect to internal floor space of the units, since 1st October 2015 national space standards have been in place as the Technical housing Space Standards. The dwellings comply to National Space standards, meeting requirements for overall area, bedroom sizes and storage allowances.

### **Security**

42. The layout of the masterplans takes into account the security required for a residential scheme. For example key three storey apartment buildings face unto the area of open space directly in the middle of the development parcel. Equally, the car parking for properties are well overlooked and dwellings located on the periphery are orientated to overlook public paths. The layout also means that there are back-to-back relationships, which restrict access to private amenity space.

### **Noise**

43. Core Strategy Policy CP1 and MDD LP Policy CC06 direct development away from areas where noise would impact upon amenity and require mitigation where noise cannot be completely avoided. Noise impacts from traffic from the A327 and NMRE upon the new occupier's amenities have been considered and are considered acceptable. Particularly with regard to the A327, the site is located behind an existing bund that shields the development from a lot of the noise produced. The

Environmental Health officer is satisfied with the proposals and therefore there is not considered to be significant impacts to the occupants of the proposed dwellings.

### **Sustainable Design and Construction**

44. Due to the replacement of the Code for Sustainable Homes with national standards in Building Regulations, the proposal is no longer required to meet Code 4 in line with Policy CC04 of the MDD. However, the policy does require that all development should incorporate suitable waste management facilities including on-site recycling (composting). Water resilience should also be built into the scheme, such as low usage toilets, showers and water recycling. It is considered that sufficient internal and external storage could be provided to accommodate this.
45. Additionally, there is a condition in the outline permission to secure low and zero carbon technologies, so to demonstrate how the development will achieve a 10% reduction in carbon emissions beyond the minimum requirement of 'Part L: Building Regulations'.

### **Access and Movement**

46. The outline planning permission established the access points to the site and was accompanied with a full Transport Assessment. This involved modelling the potential impacts of the development by using the Wokingham Strategic Transport Model which included a review of this development site, the Arborfield Garrison SDL and the wider Core Strategy development proposals. It was demonstrated that there would be no significant harm caused by additional houses to both local and the wider transport networks, subject to the delivery of the Transport Interventions that were identified and secured through a legal process. The current reserved matters application does not deviate from the established access points approved under the outline planning application. It should be noted that most of the highways related matters are being assessed in detail under separate conditions applications.
47. The main site accesses onto the Nine Mile Ride Extension were operationally assessed with the planning application for the Nine Mile Extension (reference 153336) and identified to function satisfactorily. A Road Safety Audited was also undertaken. These proposals (153336) also include pedestrian, cycle and equestrian facilities along the site frontage within the Linear Park.

### **Access and Movement – Site Layout**

48. There are three highway accesses to the site from the Nine Mile Ride Extension (NMRE), which have a direct connection to the new roundabout on the A327. These accesses from the NMRE were consented via the NMRE application (reference 153336). A 3.0 metre wide shared pedestrian and cycle route runs along the site frontage on the southern side of the NMRE. The pedestrian and cycle crossing have been modified to ensure there is an uncontrolled crossing 5-6m from the access give way line.
49. The proposal has been designed in accordance to the street hierarchy detailed in the Southern Gateway Design Code. It is composed of Primary Streets, Secondary Streets, Tertiary Streets type 1, Tertiary Streets type 2 and Access Ways / Parking Courts all designed in accordance with Manual for Streets. The Primary and Secondary Streets connect to the NMRE and provide the main connection links and are 6.1m (Primary) and 5.5m (Secondary) wide. The Tertiary Streets measure 4.8m. As buses are not planned to run through this part of the site the 4.8 metres width is

appropriate. It is considered that as these serve only a small number of units and larger vehicles are likely to meet each other very infrequently, therefore there is no significant issue in terms of highway movements.

50. There are a number of highway features that contribute to the public realm. In the central area of both parcels H and J there are 'focal squares' which comprises of traffic calming measures and differing materials that add to the character of the area and allow tertiary streets to be connected to the wider road network.
51. On the Tertiary Street, pavements have not been provided, however the proposed roads have been designed to have a shared surface for good pedestrian access within the site and future phases of development in the wider Arborfield Garrison development.
52. Additionally, the proposal includes shared surfaces and private drives off the tertiary streets that provide access to some residential dwellings. These areas of road are short in length and often consist of turning heads which will ensure low vehicle speeds within these zones. As such this is acceptable.

#### **Access and Movement – Car Parking**

53. In line with Core Strategy Policy CP6, Managing Travel Demand and MDDLDP Policy CC07: Parking, condition 40 requires reserved matters to incorporate car parking in line with the Council's standards. The standards require allocated parking to be supplemented with unallocated or visitor parking.
54. Officers are currently in discussions with the applicant regarding the detailed parking layout and therefore this information will be updated prior to committee in the members update.

#### **Access and Movement – Pedestrian, Cycle & Equine Access**

55. As discussed the proposal is located immediately off the Nine Mile Ride Extension and adjacent to Sheerlands Road. The cycle link that connects to the wider Arborfield network is located on the south side of the NMRE and therefore is adjacent to the development parcel. This cycle and pedestrian links into the parcel as well as the future bridleway on Sheerlands Road. Within the parcels H-J themselves an extensive network of walk and cycle links have been established and where there are cul-de-sacs in most instances have at least a footway connecting to other neighbouring parcels.
56. Given the proposal at Parcels H-J is the forth phase of the wider Arborfield Garrison development it is acknowledged that permeability across the wider area remains limited at this early stage. However, the applicant and the Council are working to ensure links are established early on in the development process to help promote sustainable routes across the wider area and that future residents can move between parcels and the wider area safely and easily.

#### **Access and Movement – Cycle Storage**

57. Consistent with Core Strategy Policies CP1, Sustainable Development and CP6, Managing Travel Demand, which expect development to make provision to support sustainable travel, Condition 29 of the outline planning consent requires cycle parking and storage in line with the Council's standards at the time. These are set out in MDDLDP Appendix 2: the requirement is for provision of at least one cycle

space for dwellings with three or fewer habitable rooms, two spaces for dwellings with four or five habitable rooms and three cycle spaces for larger dwellings.

58. Cycle parking is provided on site in designated individual cycle storage for dwellings and communal cycle storage for apartments. All dwellings and apartments have been provided with sufficient space for cycle storage/parking in accordance with the above requirements and as such the proposals are considered acceptable on this basis

#### **Access and Movement – Access to Public Transport**

59. In order to ensure good public transport to the site, a public transport strategy has been secured by Condition 33 of the outline planning consent. A new bus stop will be provided on the Nine Mile Ride Extension and Sheerlands Road which together mean that services will be within a 400m walk of all dwellings.

#### **Flooding and Drainage**

60. Core Strategy Policy CP1 and MDD DPD Policies CC09 and CC10 establish that new development should avoid increasing and where possible reduce flood risk and Arborfield Garrison SPD generally requires provision of a comprehensive system for water management, which takes account of existing features and includes proposals for effective sustainable urban drainage (SUDS), measures to avoid flood risk and new ponds.
61. At the outline stage, the applicant undertook a Flood Risk Assessment which determined that the residential part of the outline site is not located within areas shown to be at risk of flooding, as indicated by Flood zone 1 on the Environment Agency's Flood map. No housing development is located within areas that flood and therefore the proposal is considered acceptable in this regard.
62. Drainage was fully assessed at outline stage and further detailed assessment was undertaken as part of the development of each parcel and the Council and the EA are satisfied that the site has sufficient storage capacity and that the provision of SUDs, which will incorporate flood attenuation ponds, can be accommodated on-site and will mitigate the impacts of the development. The benefit of SUD's over the existing field use is that these should improve the current situation by preventing water from running off the land too quickly.
63. A raft of measures has been provided to alleviate flood risk for the site and to accord with the parameters set at the outline stage. These have been assessed and there is no objection from the Drainage Officer. The proposed drainage strategy generally fits in with the approved AECOM surface water drainage strategy with discharge limited to no more than what was approved at outline stage.

#### **Thames Basin Heaths Special Protection Area**

64. The Thames Basin Heaths Special Protection Area (SPA) was designated under European Directive due to its importance for heathland bird species. Core Strategy policy CP8 establishes that new residential development within a 7km zone of influence is likely to contribute to a significant impact upon the integrity of the SPA. The Arborfield Garrison SDL falls within this zone of influence and, in accordance with Core Strategy policy CP17 and Design Principle 1c (vi), mitigation in the form of Suitable Alternative Natural Greenspace (SANG) is being provided.

65. The development will be mitigated by the provision of the Suitable Alternative Natural Greenspace (SANGS) In the north-eastern part of the outline application site ("Northern SANGS") and at West Court ("West Court SANGS"). This has been designed to provide sufficient space required to compensate for the growth in resident numbers associated with this phase of the Arborfield Garrison development. Planning permission for the SANGs has been granted under the outline consent (Ref: O/2014/2280) and as such this meets the Natural England's 'Guidelines for the Creation of Suitable Alternative Natural Greenspace' (SANG) (2008) in terms of having all the essential features required to attract recreational users away from the SPA. Natural England and the Biodiversity Officer are satisfied with this approach. The SANG will be open prior to first occupation.

### **Ecology**

66. Core Strategy Policy CP7, carried forward by MDD LP Policy TB23, requires appropriate protection of species and habitats of conservation value. Design Principle 1b (i-ii) is concerned with protection of ecological habitat and biodiversity features, together with mitigation of any impacts that do arise. There is not considered to be a significant impact on ecology and therefore the proposal is considered to acceptable.

### **Archaeology**

67. Core Strategy Policy CP3 and MDD LP Policy TB25 require the archaeological impact of development to be taken into consideration. An Archaeological Evaluation report for the proposal was submitted and Berkshire Archaeology are satisfied that, given the nature of the features encountered, no further archaeological investigation will be required on this parcel.

### **Affordable and Specialist Housing**

68. Core Strategy Policy CP5 requires a mix of tenures, including up to 50% affordable housing. The Infrastructure and Contributions SPD states that development within the SDLs should seek 35% affordable housing which echoes Appendix 7 of the Core Strategy.

69. The application proposes 36 units of affordable housing on site. Under the Outline permission the s106 sought a commuted sum for the provision of off-site affordable housing in order to secure the 35% affordable housing required for the application. The onsite provision is in accordance with this agreement and therefore considered acceptable.

### **CONCLUSION**

The reserved matters are consistent with the principles and parameters established by the outline planning permission, which themselves reflect the Council's adopted policies and guidance for development within the Arborfield Garrison SDL. It is considered that the applications will deliver high quality development in accordance with the Council's spatial strategy and vision and therefore can be recommended for approval.

### **CONTACT DETAILS**

<b>Service</b>	<b>Telephone</b>	<b>Email</b>
Development Management and Regulatory Services	0118 974 6428 / 6429	<a href="mailto:development.control@wokingham.gov.uk">development.control@wokingham.gov.uk</a>

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- KEY:**
- SITE LOCATION
  - NET DEVELOPABLE AREA (5.08HA)
  - OPEN MARKET DWELLING
  - AFFORDABLE DWELLING (SHARED OWNERSHIP)
  - AFFORDABLE DWELLING (INTERMEDIATE RENTED)
  - AFFORDABLE DWELLING (SOCIAL RENTED)
  - UNALLOCATED PARKING BAY
  - VISITOR CYCLE PARKING (SHEFFIELD STAND)
  - CYCLE STORE (1.8M X 1.2M) (SECURE LOCKABLE SHED)

- ENCLOSURE DETAILS:**
- 1.8M HIGH BRICK WALL
  - 1.8M HIGH TIMBER FENCING
  - 0.45M HIGH TIMBER KNEE RAIL

- SURFACE TREATMENTS: (OR SIMILAR APPROVED)**
- MACADAM
  - TEGULA BLOCK PAVING OR SIMILAR APPROVED
  - CONTRASTING TEGULA BLOCK PAVING
  - TAR SPRAY AND CHIP SURFACE

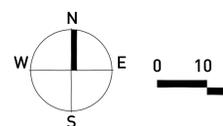
- GREEN INFRASTRUCTURE**
- PUBLIC OPEN SPACE (REFER TO LANDSCAPE PROPOSALS)
  - EXISTING TREES RETAINED (SUBJECT TO LANDSCAPE PROPOSALS)
  - TREE REMOVED TO FACILITATE DEVELOPMENT PROPOSALS (SUBJECT TO LANDSCAPE PROPOSALS)
  - INDICATIVE STREET PLANTING (SUBJECT TO LANDSCAPE PROPOSALS)
  - INDICATIVE GARDEN PLANTING (SUBJECT TO LANDSCAPE PROPOSALS)

**ACCOMMODATION SCHEDULE**

OPEN MARKET UNIT TYPE	NO. BEDS	STOREYS	NO. UNITS	SOFT/UNIT	TOTAL SOFT
FLAT 1/4 & 7	2	1 OF 3	9	626	5,634
FLAT 2/5 & 8	2	1 OF 3	9	645	5,805
FLAT 3/6 & 9	2	1 OF 3	9	753	6,777
CONISTON	2	2	3	667	2,001
LUDLOW	3	2	8	963	7,704
WARWICK	3	2	11	1059	11,649
AMBERLEY	3	2	15	1137	17,055
STRATFORD	3	2	11	1173	12,903
CAMBRIDGE (MCAT2)	3	2	10	1382	13,820
LEAMINGTON (MCAT2)	3	2	6	1417	8,502
YORK A	3	2.5	4	1468	5,872
YORK B	3	2.5	4	1453	5,812
SHAFTESBURY	4	2	17	1410	23,970
HARROGATE	4	2	16	1555	24,880
BALMORAL	4	2	3	1808	5,424
HIGHGATE 5	5	2.5	8	1932	15,456
<b>TOTAL OM</b>			<b>143</b>		<b>173,344 SOFT</b>

AFFORDABLE UNIT TYPE	NO. BEDS	STOREYS	NO. UNITS	SOFT/UNIT	TOTAL SOFT
SP2	1	2	8	444	3,552
FLAT 1/4 & 7	2	1 OF 3	3	626	1,878
FLAT 2/5 & 7	2	1 OF 3	3	645	1,935
FLAT 3/6 & 9	2	1 OF 3	3	753	2,259
CONISTON	2	2	1	667	667
TAVY (MCAT2)	2	2	3	874	2,622
TAVY NSS	2	2	6	874	5,244
TAVY 3 NSS	3	2	2	1021	2,042
DART NSS	3	2	5	1021	5,105
DART 3 NSS	3	2	1	1025	1,025
TEME (MCAT2)	4	2	1	1171	1,171
<b>TOTAL AFFORDABLE</b>			<b>36</b>		<b>29,116 SOFT</b>
<b>TOTAL DWELLINGS</b>			<b>179</b>		<b>202,460 SOFT</b>

NET DEVELOPABLE AREA	5.08 HECTARES	12.55 ACRES
COVERAGE	16.12% SOFT/ACRE	
DENSITY	35.2 DPH	
	TARGET MIX	ACTUAL MIX
	1B	4%
	2B	30%
	3B	43%
	4B	18%
	5B	9%
	100%	100%



**ARBORFIELD PARCELS H, I & J - SITE LAYOUT**



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## Alex Thwaites

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**From:** Development Control  
**Sent:** 19 April 2017 14:05  
**To:** Alex Thwaites; Planning Enquiries  
**Subject:** FW: PA 170686 - land at Arborfield Garrison, Parcels H, I and J, Arborfield  
~[UNCLASSIFIED]~

### Comments

Sinead Boyd  
Technical Assistant  
Development and Regeneration  
Development Management  
Wokingham Borough Council  
PO Box 157  
Shute End  
Wokingham  
Berkshire  
RG40 1WR

---

**From:** Arborfield & Newland Parish Council [<mailto:parishclerk@arborfield.org.uk>]  
**Sent:** 19 April 2017 12:56  
**To:** Development Control; Planning Enquiries  
**Subject:** PA 170686 - land at Arborfield Garrison, Parcels H, I and J, Arborfield

The Parish Council has no objections to this application.

Kind Regards,

Alison Ward  
Parish Clerk  
Arborfield & Newland Parish Council  
The Parish Office, Arborfield Village Hall  
Eversley Road, Arborfield, Berkshire, RG2 9PQ

0118 976 1489  
[www.arborfield.org.uk](http://www.arborfield.org.uk)

The Parish Office is open 10am - 12noon, Monday to Thursday

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## Alex Thwaites

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**From:** Clerk Barkham Parish Council <clerk@barkham-parishcouncil.org.uk>  
**Sent:** 06 June 2017 17:56  
**To:** Alex Thwaites  
**Subject:** Re: 170686 - Arborfield Garrison Parcels H, I and J ~[UNCLASSIFIED]~

No comment from barkham

Sent from Samsung Mobile

----- Original message -----

From: Alex Thwaites  
Date: 06/06/2017 13:49 (GMT+00:00)  
To: [clerk@barkham-parishcouncil.org.uk](mailto:clerk@barkham-parishcouncil.org.uk)  
Subject: 170686 - Arborfield Garrison Parcels H, I and J ~[UNCLASSIFIED]~

Afternoon,

I write with regard to application reference 170686 - Arborfield Garrison Parcels H, I and J.

Looking through my comments, I don't believe I've received any from Barkham Parish Council to date, are there any comments you wish to make regarding the application?

Kind regards,

Alex Thwaites

Senior Planning Officer

### Wokingham Borough Council

Civic Offices, Shute End, Wokingham, Berks RG40 1WR

☎ 0118 974 6431 / 07738 860 907 [Alex.Thwaites@wokingham.gov.uk](mailto:Alex.Thwaites@wokingham.gov.uk)

 Think of the environment. Please avoid printing this email unnecessarily.

*Please note, this email is an opinion of an officer of this council only which is of an advisory nature, and is given without prejudice to any formal decision taken in respect of development of the above site under the Town and Country Planning Act. Should you require any further information, please feel free to contact me on the direct dial number listed above.*

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Location	Ref No/ Planning Officer	Applicant	Proposal	Comments	Meeting date	Decision
Land at Arborfield Garrison	170686  Alex Thwaites	Redrow Homes	<p><b>Comments by 17<sup>th</sup> April</b>                      Reserved matters application pursuant to Outline planning consent O/2014/2280 for the erection of 179 dwellings with access From the Nine Mile Ride Extension (NMRE) with associated internal access roads, paths circulation areas, car parking including garages, landscaping, open space and associated infrastructure and works (Parcels H,I and J)</p>	No objection.	19.4.17	

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# Agenda Item 20.

<b>Development Management Ref No</b>	<b>No weeks on day of committee</b>	<b>Parish</b>	<b>Ward</b>	<b>Listed by:</b>
171328	9	Wargrave	Remenham, Wargrave and Ruscombe	Scheme of delegation

**Applicant** Oraclemarker Ltd.

**Location** 15-27, High Street, Wargrave. **Postcode** RG10 8BU.

**Proposal** Full planning application for demolition of existing car showroom, repair/MOT garage plus "Suncroft" dwelling and proposed erection of 10 dwellings with provision of car-parking, private amenity space, bin and bicycle storage.

**Type** Minor  
**PS Category** 13  
**Officer** Mark Croucher

**FOR CONSIDERATION BY** Planning Committee on 12<sup>th</sup> July 2017  
**REPORT PREPARED BY** Head of Development Management and Regulatory Services

## SUMMARY

The application proposes the erection of 10 dwellings (6 x 2 bed houses, 2 x 2 bed flats and 2 x 3 bed duplex units) following the demolition of existing buildings at Wargrave Motors and the property known as Suncroft.

The application site is on a prominent corner at the entrance to Wargrave and is within a Conservation Area. It is within a Modest Development Location close to local facilities and service and it is considered that the principle of residential development is acceptable in such areas. The proposal will result in the loss of employment land and the planning policy team has raised no objection to this, partly because the existing buildings detract from the character and appearance of the Conservation Area.

The Conservation Officer has recommends the application for approval and the design of the scheme will enhance the character and appearance of the Conservation Area. The development will have an acceptable impact on the amenity of the occupiers of the neighbouring dwellings. It is considered that sufficient parking will be accommodated into the scheme and that no detrimental highway safety implications will arise. Sufficient areas of soft landscaping and adequate garden sizes would be incorporated.

The proposed development is considered to be acceptable in all aspects and compliant with the relevant national and development plan polices. The application is accordingly recommended for approval subject to conditions.

## PLANNING STATUS

- Modest Development Location
- Area of high archaeological potential.
- Conservation Area.

- Local Centre.
- Green Route.

## RECOMMENDATION

APPROVAL, subject to the following conditions:

### Time Limit

1. The development hereby permitted shall be begun before the expiration of three years from the date of this permission.

*Reason: In pursuance of s.91 of the Town and Country Planning Act 1990 (as amended by s.51 of the Planning and Compulsory Purchase Act 2004).*

### Approved plans

2. This permission is in respect of the submitted application plans and drawings numbered ref: 16-P1382-200; 16-P1382-200.1; 16-P1382-201; 16-P1382-202; 16-P1382-203; 16-P1382-204; 16-P1382-205; 16-P1382-LP; 16-P1382-CP; 16-P1382-105; 16-P1382-107 and P596/001 Rev A. The development shall be carried out in accordance with the approved details unless otherwise agreed in writing by the Local Planning Authority.

*Reason: For the avoidance of doubt and to ensure that the development is carried out in accordance with the application form and associated details hereby approved.*

### Material Samples/details

3. Before the development hereby permitted is commenced, samples and details of the materials to be used in the construction of the external surfaces of the building/s shall have first been submitted to and approved in writing by the local planning authority. Development shall not be carried out other than in accordance with the so-approved details.

*Reason: To ensure that the external appearance of the building is satisfactory.*

*Relevant policy: Core Strategy policies CP1 and CP3.*

4. Before the development hereby permitted is commenced, further details showing how the existing brick/flint building is to be adapted for use as a cycle and bin store shall first be submitted to and approved in writing by the local planning authority. Development shall not be carried out other than in accordance with the so-approved details.

*Reason: In the interests of visual amenity and to safeguard the character of the conservation area in which the development is located.*

### Trees & Landscaping

5. Prior to the commencement of the development there shall be submitted to and approved in writing by the local planning authority a scheme of landscaping, which shall specify species, planting sizes, boundary treatment, tree pits spacing and numbers of trees/shrubs to be planted, and any existing trees or shrubs to be retained. Details of boundary treatment and hard landscaping shall also be included. Planting shall be carried out in accordance with the approved details in the first planting and seeding seasons following the occupation of the building(s).

Any trees or plants which, within a period of 5 years from the date of the planting (or within a period of 5 years of the occupation of the buildings in the case of retained trees and shrubs) die, are removed or become seriously damaged or diseased shall be replaced in the next planting season with others of similar size and species or

otherwise as approved in writing by the local planning authority.

*Reason: To ensure adequate planting in the interests of visual amenity. Relevant policy: Core Strategy policy CP3 and Managing Development Delivery Local Plan policies CC03 and TB21 (and TB06 for garden development)*

6. No trees, shrubs or hedges within the site which are shown as being retained on the approved plans shall be felled, uprooted wilfully damaged or destroyed, cut back in any way or removed without previous written consent of the local planning authority; any trees, shrubs or hedges removed without consent or dying or being severely damaged or becoming seriously diseased within 5 years from the completion of the development hereby permitted shall be replaced with trees, shrubs or hedge plants of similar size and species unless the local planning authority gives written consent to any variation.

*Reason: To ensure adequate planting in the interests of visual amenity. Relevant policy: Core Strategy policy CP3 and Managing Development Delivery Local Plan policies CC03 and TB21 (and TB06 for garden development)*

### **Highway safety and parking**

7. No part of any building(s) hereby permitted shall be occupied or used until vehicle parking and turning space has been provided in accordance with details to be submitted to and approved in writing by the local planning authority. The vehicle parking and turning space so-approved shall be retained in accordance with the approved details and the parking space remain available for the parking of vehicles at all times and the turning space shall not be used for any purpose other than vehicle turning.

*Reason: In the interests of highway safety and convenience. Relevant policy: Core Strategy policies CP3 & CP6.*

8. No building shall be occupied until secure and covered parking for cycles has been provided in accordance with the approved drawing(s)/details. The cycle parking/storage shall be permanently so-retained for the parking of bicycles and used for no other purpose.

*Reason: In order to ensure that secure weather-proof bicycle parking facilities are provided so as to encourage the use of sustainable modes of travel. Relevant policy: NPPF Section 4 (Sustainable Transport) and Core Strategy policies CP1, CP3 & CP6 and Managing Development Delivery Local Plan policy CC07.*

9. The dwelling/building shall not be occupied until visibility splays of 2.0 metres by 2.0 metres, have been provided at the intersection of the driveway and the adjacent footway. (Dimensions to be measured along the edge of the drive and the back of the footway from their point of intersection). The visibility splays shall thereafter be kept free of all obstructions to visibility above a height of 0.6 metres.

*Reason: In the interests of highway safety and convenience. Relevant policy: Core Strategy policies CP3 & CP6.*

10. No building shall be occupied until the vehicular access has been surfaced with a permeable and bonded material across the entire width of the access for a distance of 10 metres measured from the carriageway edge.

*Reason: To avoid spillage of loose material onto the highway, in the interests of road safety. Relevant policy: Core Strategy policy CP6.*

11. The existing vehicular access(es) to the site shall be stopped up and abandoned,

and the footway and/or verge crossings shall be re-instated within one month of the completion of the new access(es) in accordance with details to be submitted to and approved in writing by the local planning authority.

*Reason: In the interests of highway safety and convenience. Relevant policy: Core Strategy policies CP3 & CP6.*

12. No development shall take place, including any works of demolition, until a Construction Method Statement has been submitted to, and approved in writing by, the local planning authority. The approved Statement shall be adhered to throughout the construction period. The Statement shall provide for:

- i) the parking of vehicles of site operatives and visitors,
- ii) loading and unloading of plant and materials,
- iii) storage of plant and materials used in constructing the development,
- iv) the erection and maintenance of security hoarding including decorative displays and facilities for public viewing, where appropriate,
- v) wheel washing facilities,
- vi) measures to control the emission of dust and dirt during construction,
- vii) a scheme for recycling/disposing of waste resulting from demolition and construction works.

*Reason: In the interests of highway safety & convenience and neighbour amenities. Relevant policy: Core Strategy policies CP3 & CP6.*

13. Prior to the occupation of the dwellings hereby approved, a parking schedule shall be submitted to and approved in writing by the local planning authority setting the allocated and unallocated spaces unless otherwise first agreed in writing by the local planning authority.

*Reason: to ensure a satisfactory level of parking and parking layout.*

### **Drainage**

14. No development shall take place until full details of the drainage system for the site have been submitted to and approved in writing by the LPA. The details shall include:

- Results of intrusive ground investigation demonstrating seasonal high groundwater levels for the site and infiltration rates in accordance with BRE365.
- Demonstration that the base of SuDS features are at least 1m above seasonal groundwater level.
- Full calculations demonstrating the performance of soakaways or capacity of attenuation features to cater for 1 in 100-year flood event with a 40% allowance for climate change and runoff controlled at Greenfield rates, or better.
- Calculations demonstrating that there will be no flooding of pipes for events up to and including the 1 in 100 year flood event with a 40% allowance for climate change.
- A drainage strategy plan for the proposed development, including pipe details with invert levels.
- A maintenance management plan for the SuDS features throughout the lifetime of the development, as well as who will be responsible for the maintenance.

*Reason: This is to prevent increased flood risk from surface water run-off.  
Relevant policy: NPPF Section 10 (Meeting the Challenge of Climate Change, Flooding and Coastal Change), Core Strategy policy CP1 and Managing Development Delivery Local Plan policies CC09 and CC10.*

### **Environmental Health**

15. No work relating to the development hereby approved, including works of demolition or preparation prior to building operations, shall take place other than between the hours of 8:00 am and 6:00 pm Monday to Friday and 8:00 am to 1.00pm. Saturdays and at no time on Sundays or Bank or National Holidays.

*Reason: In the interests of the amenities of neighbouring occupiers.*

16. Before development commences the applicants shall submit for written approval to the Local Planning Authority a scheme of works that sets out the measures that will be taken to minimise dust arising from the development. The dust mitigation measures identified in the scheme shall be carried out and maintained until construction is complete.

*Reason: To protect the amenity of local residents during the construction period.*

17. Prior to the commencement of development, unless otherwise agreed by the local planning authority, development other than that required to be carried out as part of an approved scheme of contamination remediation must not commence until conditions A – D (below) have been complied with. If unexpected contamination is found after development has commenced, development must be halted on that part of the site affected by the unexpected contamination, to the extent specified in writing by the Local Planning Authority, until there is compliance with condition D (below)

a) *Site Characterisation*

An investigation and risk assessment shall be completed in accordance with a scheme that has been submitted and approved in writing by the local planning authority to assess the nature and extent of contamination on the site, whether or not it originates at the site. (This is in addition to any assessment that may have been provided with the planning application) The investigation and risk assessment must be undertaken by competent persons and a written report of the findings must be submitted to the local planning authority for approval. The report of the findings must include:

- (i) a survey of the extent, scale and nature of the contamination;
- (ii) an assessment of the potential risks to:
  - (a) human health;
  - (b) property (existing or proposed) including buildings, crops, livestock, pets, woodland and services and pipework;
  - (c) adjoining land;
  - (d) groundwater and surface waters;
  - (e) ecological systems;
  - (f) archaeological sites and ancient monuments
- (iii) an appraisal of remedial options, and proposal of the preferred option (N.B. The assessment must be conducted in accordance with DEFRA and the Environment Agency's 'Model Procedures for the Management of Land Contamination CLR11.)

b) *Submission of a remediation scheme*

A detailed remediation scheme that describes how the site will be made suitable for the intended use must be submitted to the local planning authority for written approval. The remediation scheme shall include, the proposed remediation objectives and remediation criteria, details of all works to be undertaken, the timetable of works and site management procedures. The remediation scheme shall ensure that the site cannot be declared as being contaminated under part 2A of the Environmental Protection Act 1990, in relation to the intended use, after remediation works are completed.

c) *Implementation of the approved remediation scheme*

The approved remediation scheme shall be implemented before other groundworks or construction works commence unless a phased approach has been agreed as part of the approved remediation scheme or unless written approval is given by the Local Planning Authority. The applicant or contractor must give at least two weeks written notice before remediation works commence. Following completion of remediation works at the site, or upon completion of each phase a verification report shall be submitted to the Local Planning Authority for written approval.

d) *Reporting of Unexpected Contamination*

If unexpected contamination is found at any time during development this shall be reported in writing as soon as possible to the Local Planning Authority. An investigation and risk assessment shall be carried out in accordance with the requirements of condition A (above), and where remediation work is necessary a remediation scheme must be prepared and submitted for written approval to the local planning authority, in accordance with condition B (above). Following the completion of measures set out in the approved remediation scheme a verification report shall be submitted to the local planning authority in accordance with condition C.

*Reason: to ensure that any contamination of the site is identified at the outset to allow remediation to protect existing/proposed occupants of property on the site and/or adjacent land.*

**Permitted development rights**

18. Notwithstanding the provisions of Classes A, B and E of Part 1 of the Second Schedule the Town and Country Planning (General Permitted Development) (England) Order 2015 (or any Order revoking and re-enacting that Order with or without modification), no buildings, enlargement or alterations permitted shall be carried out without the express permission in writing of the local planning authority.

*Reason: to maintain a continuity of the development and preserve the character and appearance of the area.*

19. Notwithstanding the provisions of Classes E of Part 1 of the Second Schedule the Town and Country Planning (General Permitted Development) (England) Order 2015 (or any Order revoking and re-enacting that Order with or without modification), no additional hard surfacing, other than depicted in the plans hereby approved, shall be laid to between the front elevation of the dwellings and the highway unless otherwise agreed in writing by the Local Planning Authority.

*Reason: To ensure the soft landscaping to the front of the site maintained and to preserve the character and appearance of the area.*

20. The side windows in the development hereby permitted shall be fitted with obscured glass and shall be permanently so-retained. The window shall be non-opening unless the parts of the window, which can be opened, are more than 1.7 metres above the finished floor level of the room in which the window is installed and shall be permanently so-retained.

*Reason: to avoid overlooking into neighbouring dwellings and a loss of privacy.*

### **Archaeology**

21. No development shall take place within the site until the applicant, or their agents or their successors in title, has secured and implemented a programme of archaeological work (which may comprise more than one phase of work) in accordance with a written scheme of investigation, which has been submitted to and approved in writing by the Local Planning Authority. The development shall only take place in accordance with the detailed scheme approved pursuant to this condition.

*Reason: To ensure that any archaeological remains within the site are adequately investigated and recorded or preserved in situ in the interest of protecting the archaeological heritage of the borough.*

### **Ecology**

22. Prior to commencement of the development, a detailed scheme to provide wildlife enhancements for birds and bats shall be submitted to and approved in writing by the local planning authority. The measures contained within the plan shall be implemented in accordance with the approved plan unless otherwise approved in writing by the local planning authority.

*Reason: To secure wildlife enhancements within the course of the development, as appropriate under the NPPF.*

### **Informatives**

1. The Local Planning Authority has acted positively and proactively in determining this application by assessing the proposal against all material considerations, including planning policies and any representations that may have been received. This planning application has been the subject of positive and proactive discussions with the applicant in terms of addressing concerns relating to highway safety and residential amenities. The decision to grant planning permission in accordance with the presumption in favour of sustainable development as set out in the NPPF is considered to be a positive outcome of these discussions.
2. The Head of Highways at the Council Offices, Shute End, Wokingham [0118 9746000] should be contacted for the approval of the access construction details before any work is carried out within the highway (including verges and footways). This planning permission does NOT authorise the construction of such an access or works.
3. Adequate precautions shall be taken during the construction period to prevent the deposit of mud and similar debris on adjacent highways. For further information contact the Highway Authority on tel.: 0118 9746000.
4. Any works/ events carried out by or on behalf of the developer affecting either a public highway or a prospectively maintainable highway (as defined under s.87 New

Roads and Street Works Act 1991 (NRSWA)), shall be co-ordinated and licensed as required under NRSWA and the Traffic Management Act 2004 in order to minimise disruption to both pedestrian and vehicular users of the highway. Any such works or events, and particularly those involving the connection of any utility to the site must be co-ordinated by the developer in liaison with the Borough's Street Works team (0118 974 6302). This must take place at least three months in advance of the intended works to ensure effective co-ordination with other works so as to minimise disruption.

5. The development hereby permitted is liable to pay the Community Infrastructure Levy. This is a matter for the developer. The Liability Notice issued by Wokingham Borough Council will state the current chargeable amount. Anyone can formally assume liability to pay, but if no one does so then liability will rest with the landowner. There are certain legal requirements that must be complied with. For instance, whoever will pay the levy must submit an Assumption of Liability form and a Commencement Notice to Wokingham Borough Council prior to commencement of development. For more information, see <http://www.wokingham.gov.uk/planning/developers/cil/cil-processes/>.
6. The applicant is advised to consider the rubbish and recycling information on the Council's website: <http://www.wokingham.gov.uk/rubbish-andrecycling/collections/information-for-developers/>.
7. Should any bats or evidence of bats be found prior to or during the development, all works must stop immediately and an ecological consultant or the Council's ecologist contacted for further advice before works can proceed. All contractors working on site should be made aware of the advice and provided with the contact details of a relevant ecological consultant.

#### **PLANNING HISTORY**

170129: Full application for the demolition of existing car showroom and repair/MOT garage and associated structures and their replacement with 8 new dwellings (6x2-bed and 2x3-bed), demolition of dwelling known as "Suncroft" and replacement with a detached building comprising 2x1-bed flats and 1x2-bed dwelling, provision of off-street car parking, private amenity space, bin and bicycle storage. Withdrawn: 04/05/2017.

#### **SUMMARY INFORMATION**

Site Area	0.26 hectares
Previous land use(s) and floorspace(s)	Vehicle sales and repairs (B2 & sui generis)
Existing units	1
Proposed units	10
Existing parking spaces	40
Proposed parking spaces	17

#### **CONSULTATION RESPONSES**

**WBC Policy and Plans** – No objection.

**WBC Conservation Officer** – No objection.

**WBC Drainage** – No objection subject to condition 14.

**WBC Environmental Health** – No objection subject to conditions 15 - 17.

**WBC Highways** – No other objection subject to conditions 7 -13.

**WBC Trees and Landscape** – No objection subject to condition 5 and 6.

**WBC Biodiversity** – No objection subject to condition 22.

**Archaeology** – No objection subject to condition 21.

**Affordable Housing/Viability** – No objection. It is not viable to provide and affordable housing contribution.

**Cllr Halsall** - list on the grounds of inappropriate development in the conservation area in particular with its intensity. I believe that vehicles will spill out into the High Street and Wargrave Hill and that vehicles will not be able to suitably manoeuvre in the site to access rear buildings.

**Parish Council** - proposal was situated in a locality of pre-existing car parking issues and therefore, there was inadequate parking provision for the practicality of the site (including visitor or delivery):

- Inadequate turning arrangements for the rear element of the proposal (especially deliveries/collection of refuse);
- The overall size site density of the proposal is out of keeping with the conservation area locality and therefore represents an over development of residential development.
- The rear element of the proposal may lead to an intensification of activity impacting upon highway safety.
- The proposal lacks affordable housing provision on site.

## REPRESENTATIONS

### 11 letters received.

4 letters **objecting** to the development and 6 letters making both negative and positive comments. **Objecting points raised (in summary):**

- Overdevelopment. (paragraphs 9 – 29, 31, 34 - 36)
- Access points are very busy. (paragraphs 30 - 34)
- Rear access is on a blind bend. (paragraphs 30 - 34)
- Loss of privacy to neighbouring houses. (paragraphs 23 & 25)
- The increase in the use of the track to the rear will result in greater conflict with pedestrians. (paragraph 33)
- The track to the rear is in a poor state of repair. (paragraph 33)
- Overlooking. (paragraphs 23 & 25)
- Overbearing impact on neighbouring properties. (paragraphs 22 – 29)
- The development is too large. (paragraphs 9 – 29, 31, 34 - 36)
- The trees on the site will not adequately screen the development. (paragraphs 25, 39 – 40)
- Insufficient parking. (paragraph 35 – 37)

- There are few amenities in the area. (paragraph 1 & 36)
- There is no turning for delivery vehicles on the northern part of the site. (paragraph 31)
- No space for construction vehicles to park. (condition 12)
- There is no pavement on Wargrave Hill. (paragraph 34)
- The flat roof dormer window is not in keeping with other dormers in the area. (paragraph 17)
- UPVC fascia and soffits are not in keeping with the Conservation Area. (condition 3)
- Parking is a problem in the area. (paragraph 35)
- The density is too high. (paragraphs 10 – 12)

1 Letter of **support** and 6 other letters making both positive and negative comments.

**Supporting points raised (in summary):**

- It will improve the appearance of the village.
- The scheme looks good.
- The development will enhance the Conservation Area.
- Supportive of 'less expensive' properties in Wargrave.

**APPLICANTS POINTS**

- The new dwellings are of a high quality, traditional design that is reflective of the Conservation Area within which it is located.
- The proposed development will enhance the character and appearance of the Conservation Area.
- Parking is provided within a courtyard to the front of the building, this parking area will be enclosed by new landscaping which will positively enhance the area. No landscaping currently exists to the front of the site to screen the area used to display cars for sale.
- The proposed layout has been influenced by the adjoining houses and in particular, the need to take account of their amenity.
- The proposed buildings are staggered which breaks up the visual bulk of the proposal.
- The proposed buildings have been sensitively sited with habitable room windows located away from sensitive boundaries to ensure that no material overlooking occurs.
- The new bin and cycle store will be formed by incorporating an element of the original garage brick and flint building.
- The application site falls within a highly sustainable location where the principle of redevelopment receives strong support from the NPPF and relevant adopted planning policies.

**PLANNING POLICY**

NPPF

- Chapter 1: Building a strong, competitive economy
- Chapter 4: Promoting sustainable transport
- Chapter 6: Supporting a prosperous rural economy
- Chapter 7: Requiring good design
- Chapter 10: Meeting the challenge of climate change, flooding and coastal change
- Chapter 11: Conserving and enhancing the natural environment
- Chapter 12: Conserving and enhancing the historic environment

Core Strategy	<p>CP1 Sustainable Development  CP2 Inclusive communities  CP3 General Principles for Development  CP5 Housing mix, density and affordability  CP6 Managing travel demand  CP7 Biodiversity  CP9 Scale and location of development proposals  CP17 Housing delivery</p>
MDD Local Plan	<p>CC01 Presumption in Favour of Sustainable Development  CC02 Development Limits  CC03 Green Infrastructure, Trees and Landscaping  CC04: Sustainable Design and Construction  CC06 Noise  CC07 Parking  CC09 Development and Flood Risk (from all sources)  CC10 Sustainable Drainage  TB05 Housing Mix  TB06: Development of Private Residential gardens  TB07: Internal Space Standards  TB15: Major Town, and Small Town/ District Centre Development  TB21 Landscape Character  TB23: Biodiversity and Development  TB24: Designated Heritage Assets (Listed Buildings, Historic Parks and Gardens, Scheduled Ancient Monuments and Conservation Areas)  TB25: Archaeology</p>

## PLANNING ISSUES

### Principle of development

1. The site is located within development limits of Wargrave, which is a Modest Development Location. The Core Strategy sets out that *'Modest Development Locations are those with access to some facilities and services either within them or through good public transport services to major development locations or centres in neighbouring areas'* and that residential developments up to 25 units is appropriate in such areas. The proposed development seeks the erect of 10 residential units resulting in a net gain of 9 dwellings. The scale and quantum of development proposed is acceptable in Modest Development Locations such as Wargrave.
2. The site is not located within a designated Core Employment Area. The proposed development would result in the loss of 168.4m<sup>2</sup> of Class B2 floor space (General Industrial) from the MOT/repair garage and 296m<sup>2</sup> of class Sui Generis floorspace.
3. Core Strategy Policy CP15 (Employment Development) states: 'Any proposed change of use from B1, B2 and B8 should not lead to an overall net loss of floor-space in B Use within the Borough'. Paragraph 4.70 to policy CP15 of the Core Strategy refers to an Employment Land Study (2005) which sets out that the supply of floor space for industry and warehousing needs to increase by 51,000m<sup>2</sup> to meet forecast B use growth in the Borough over the Plan period to 2026.

4. The Employment Land Monitoring Report for April 2013 – March 2014 demonstrates that there has been a net gain of 3,649m<sup>2</sup> of B use floor space in the borough since 1st April 2006. The report also shows that the borough has 88,099m<sup>2</sup> of extant permissions for B use floorspace in the monitoring year, which could be completed in the next six years based on historic completion rates. It should be noted that the 3,649m<sup>2</sup> figure is based on data that is almost three years old and it is considered highly likely that this figure has since been further eroded.
5. The Central FEMA (Functional Economic Market Area) Economic Development Needs Assessment (EDNA) report has been published (October 2016) which identifies a recommended net manufacturing (B1c/B2) space requirement for 2013-2036 of at least 11,655m<sup>2</sup> based on the labour supply approach. This would suggest the need to retain employment land in any future local plan strategy and therefore careful consideration should be given before permitting loss of employment floorspace. It is however noted that this study has not factored in the allocated Science Park south of the M4, which is expected to deliver significant amounts of employment floorspace up to and beyond the current plan period.
6. Notwithstanding the above, it is noted that the site lies on the edge of the local centre of Wargrave on the high street, within the conservation area. There are houses between the subject site and the main centre retail units, creating a separation within the current local centre boundary area.
7. The planning policy team has considered the information set out above and raised no objection:

*'On balance, in this highly prominent location in the conservation area, there should be a consideration of a more suitable use and form of development. As such, there is no policy objection in principle to the proposed development in the context and setting, subject to Conservation Officer confirming that this would lead to the enhancement of the conservation area. Marketing information to show that there is no requirement/demand for the existing use is therefore not required in this case.'*

8. The loss of employment and proposed residential re-development of the site is therefore acceptable in principle subject to the material considerations set out below.

#### **Design and impact on character and appearance of the Conservation Area**

9. The site is in a prominent location on the corner of Wargrave Hill and the High Street and forms part of the gateway into the centre of the village. The proposed building will replace a garage, sales forecourt and large canopy structure, which is considered to detract from the historic character and appearance of the Conservation Area. It is therefore considered that the proposed development will vastly improve the visual amenity of the locality and the rationale for this conclusion is set out in further detail in this section of the report.
10. The proposed development will have a density of 38 dwellings per hectare (dph). The density of the existing area varies due to the mix of property types in close proximity to the site and the clustered & irregular historic settlement pattern. The row

of terrace properties along the high street to the south east has a density of approximately 80 dwellings per hectare. Whereas the properties immediate to the south of the site have a density of 20 dwellings per hectare. Larger dwellings at the edge of the village have a density of approximately 5 dwellings per hectare.

11. Given the irregular development pattern of Wargrave and the broad range of property sizes and types close to the application site, the dph ratio is not a particular useful tool in assessing the proposed development in this case. Notwithstanding this, it is considered that the dph of the site is keeping with the character of the settlement and is commonly found in built up areas.
12. The proposed building to the front of the site will be in keeping with the grain of development along the High Street. It will respect the existing building line and have a staggered form to provide visual interest and replicate the gable features that characterise the area. The footprint of the building to the frontage will have a similar depth and form to the other buildings in the locality.
13. The properties located to the rear of the site will replace an existing bungalow and it is therefore not out of keeping with the existing settlement pattern to have residential development in this location. There are also of examples of buildings set back behind the properties along the High Street.
14. The proposed development includes four flats, which have been designed to appear as one large property. This is entirely in keeping with the area, which includes large houses that have been subdivided into flats. Wargrave Hall to the west of the site is one such example of this.
15. The proposed dwellings along the road frontage will be approximately 9.7 metres in height and will in fact be marginally lower than the neighbouring properties to the south of the site, which are approximately 10.45 metres high. As aforementioned, the depth and footprint of the properties is reflective of the area.
16. The elevational design utilises Edwardian and Victorian design features. The Arts & Craft movement spans from the late Victorian period to the early 20<sup>th</sup> century and the proposed development also incorporates details that are associated with this architectural movement. The proposal will utilise design features such as: tile hanging; rough cast render; solid arch detailing; prominent string course; gable features; steep roof pitches; finial detailing, and sash & casement windows. The development will also utilise brick and flint to reflect existing buildings on the site (proposed to be retained and used as a cycle and bin store) and local building materials. The bays and proportions of windows to masonry of the buildings will also complement the existing properties in the locality.
17. A neighbour representation has criticised the use of flat roof dormer windows. These features are characteristic of historic buildings and are common found on Georgian properties: There is a prominent example of this on a building located at the main cross roads in the centre of the village. There are also other examples of flat roof dormer windows on buildings along Church Street and School Lane. Whilst the proposed building is not replicating a Georgian style, Arts & Crafts properties utilised traditional buildings techniques and flat roof dormer windows were not uncommon. The proposed dormer windows will be proportionately small features positioned on the middle of the roof slope. It is considered they will be elegant features to the

building and have a satisfactory appearance.

18. The application site is within a Conservation Area. Policy TB24 of the MDD Local Plan states the Borough Council will conserve and seek the enhancement of designated heritage assets in the Borough and their settings by *'supporting development proposals or other initiatives that will conserve and, where possible, enhance the local character'*.
19. The Conservation Officer has made the following comments and recommend the application for approval:

*'Whilst representing an increase in the density of residential development on this prominent corner, the removal of the unsightly garage forecourt and canopy weigh in its favour.*

*The existing modern garage buildings are of no historic or architectural interest and their removal is welcome. To the northern boundary of the site are an attractive brick and flint wall and a number of older buildings, which have historic interest as evidence of the former use of this site. Both the wall and the older of these buildings are now to be retained, the latter proposed for use as a store for cycles/bins.*

*The northern corner of the development has also be revised following comments that the swept curve design of the building, around the corner to face Wargrave Hill, had little precedent in the village. The more frequent use of gables to turn a corner in the village is now reflected in the proposed building.*

*To the rear of the site, a unit has been removed to reduce the density and cramped feel of this part of the development.*

*The materials proposed are consistent with those traditional materials used locally (red/orange brick and flint, clay tiles and painted timber windows and doors).*

*Overall, subject to conditions, the revised scheme is considered to preserve the character of the conservation area and setting of nearby listed buildings.'*

(it should be noted that the reference to previous proposals in the above comments refer to the withdrawn application ref: 170129)

20. For the reasons set out above, it is considered that the proposed development will enhance the character and appearance of the Conservation Area.

#### **Amenity of future residents**

21. The gardens to the residential dwellings will be at least 11 metres deep and will meet the minimum garden depth requirement set out in the Borough Design Guide SPD. The proposed flats will have a communal outdoor space that is considered commensurate with the size of the apartments. All of the properties will meet the nationally described space standards. The development will therefore result in a

satisfactory level of amenity for the future occupiers.

## **Impact on Neighbours**

### *The Garth*

22. The rear elevation of the neighbouring property known as 'the Garth' faces the application site. It will be at an oblique angle to the proposed building along the frontage and would be approximately 18 metres from the corner of the structure. The Borough Design Guide SPD recommends a back to flank separation distance of 12 metres and the proposed development will achieve this. Whilst the building is two storeys in height, there is accommodation in the roof space and therefore it is also appropriate to consider recommended separation distances for buildings over 2 storeys: The Borough Design Guide recommends a 15 metres gap from rear to flank and the development will also achieve this measurement.
23. The Garth is situated on higher land than the proposed building to the front of the site and this will lessen the impact of the development to the occupiers of this property. None of the upper floor windows in the side (north) elevation of the proposed building will serve as the principal windows to habitable rooms and condition 20 is recommended ensuring they are obscurely glazed.
24. The Garth does not directly back onto the rear elevation of the proposed building and therefore the recommended back-to-back separation distances are not entirely relevant in this case. However, it is useful to consider these as the buildings would be at an oblique angle to each other. To avoid overlooking the Borough Design Guide SPD recommends a back-to-back separation distance of 22 metres for 2 storey windows and 30 metres for windows above this. These separation distances assume the common boundary is equidistant between both properties. Therefore, rear windows should be at least 11 metres (2 storey) and 15 metres (over 2 storeys) from the common boundary. The centre point of the rear windows in the proposed building will be at least 11 and 15 metres from the common boundary with The Garth. The rear windows of the proposed building are also orientated to look directly out onto the application site.

### *29 High Street*

25. Plots 9 and 10 will be located to the rear of the site and will replace the existing bungalow known as Suncroft. Both properties will have gardens 11 metres deep and will comply with the Borough Design Guide SPD. The gardens will share a common boundary with the rear garden of no. 29 High Street. This property has a relatively long rear garden measuring 34 metres with trees along the common boundary. It is considered that plots 9 and 10 will have an acceptable impact on the amenity of the neighbouring occupants given the size of the neighbouring garden, tree screening and the distance of these proposed dwellings from the boundary.
26. Plot 1 will be set 3.4 metres from the side elevation of 29 High Street and this complies with the separation distances in the Borough Design Guide SPD. This separation distance is also considered to be appropriate taking into account the tight grain of development that characterises Wargrave. Plot 1 will project approximately 2 metres beyond the rear of this neighbouring property but this will not detrimentally affect light or outlook to the neighbouring rear windows or garden area.

27. 29 High Street has 2 first floor side windows that face the application site. One of the windows serves as the principal window to a bedroom. The outlook of the bedroom is orientated towards the roof of the car sales building and it is considered that there would not be a detrimental impact to the outlook of this room. With regard to a loss of light, the centre point of this window will not face the highest point of the roof and the total 3.4 metre separation distance to the proposed building will be sufficient for this room to afford natural light. The bedroom is not a main room to the house or the master bedroom. It is not considered that the impact to this window would weigh in the balance of refusing the proposed development.

#### *Other properties*

28. The proposed development will be suitably located away from any other residential dwellings as to not detrimentally impact the amenity of the respective occupiers.

29. No objection is raised with regard to the impact of the development on neighbouring properties.

#### **Highways and parking**

##### *Access / safety*

30. The existing access to the site is located on the Junction of the High Street and Wargrave Hill. The proposal development will stop up this access and move it further away from the junction. There is an existing dropped kerb in the location where the proposed access will be located and it appears that there may have once been an entrance in this position. Moving the access further away from the junction will improve highway safety. It is considered that the proposed access would achieve satisfactorily visibility splays.

31. The submitted Transport Statement includes tracking diagrams for both parking areas showing that there is sufficient space for vehicles (including delivery vehicles) to manoeuvre on the site and enter and leave the highway in a forward gear.

32. A traffic survey has been undertaken as part of the Transport Statement. The survey found that there was an average of 74 vehicle movements per day to and from the existing garage on the site. The TRICS data suggests a daily traffic generation rate of 4.283 trips per dwelling. The resulting daily traffic generation will therefore be 42 movements which is a significant reduction from the observed 74 movements per day from the existing garage. The reduction in traffic generation from the site is a significant benefit to road safety and weighs in favour of the proposed development.

33. There will be an access to the rear of the site via a public Right of Way. This is already used by vehicular traffic associated with The Garth; Suncroft; Woodclyffe and 5 Wargrave Hill. The vehicle access section of the Right of Way is privately owned and the surface is comparable to other private roads in the borough. The Highway Officer has raised no objection to one additional dwelling using this access. The Public Right of Way Officer also raises no objection.

34. A neighbour representation has commented on the fact there is no footpath along the lower part of Wargrave Hill. The Highway Engineer has advised that due to the

width of the carriageway and the neighbouring land, it is not feasible for a footpath to be retrofitted in this location. It is considered that the majority of footfall from the development would be onto the High Street towards the village centre. The properties to the rear will be able to access the centre via a public right of way that runs parallel to the eastern boundary.

### *Parking*

35. There will be 17 parking spaces on the site serving the proposed dwellings. The Highway Engineer has advised that '*the parking standards for this development would be 14 spaces (10 allocated 2 unallocated and 2 visitor spaces) or 11 spaces (3 allocated 6 unallocated and 2 visitor spaces).*' The proposed parking is therefore well in excess of the Council's parking standards. Each of the proposed dwellings will have secured cycle parking facilities and this will be secured by condition 8. Furthermore, the site is located within a sustainable location and the future occupiers will be able to access local facilities and service without the reliance on private motor vehicles. A bus stop is located directly outside of the site with services to Reading; Henley; High Wycombe and Twyford.
36. Several representations have raised concerns regarding parking on the site and the local area. The development will exceed the councils parking standards and is located within a sustainable settlement with access to local facilities, service and public transport. Taking these factors into consideration, no objection can be raised regarding insufficient parking capacity.
37. The Highway Engineer has not raised any objections regarding highway safety implications and the residential redevelopment of the site will result in highway safety improvements. There will be no detrimental conflict between cars manoeuvring on to- and off- of the site and traffic movements along Wargrave Hill or the High Street. It is considered that the proposed development has an acceptable level of parking and will not result in any detrimental highway safety impacts subject to conditions 7 - 13.

### **Trees and Landscaping**

38. The Tree and Landscape Officer has advised that there is a slight conflict relating to the footprint of plot 10 coming slightly into the root protection area (RPA) of a Beech tree to the east. However, the incursion into the RPA will be approximately 2% and this will not be an issue now or in the future with continued growth of the tree.
39. A 'Landscape General Arrangement Plan' has been submitted and this provides an indication of design intent with the tree and shrub species. The front of the site will be punctuated by elements of soft landscaping to break up the hard surfacing. This will be an improvement on the existing garage forecourt, which contains no landscaping. The proposed soft landscaping will also be more generous than many of the other properties along the High Street that are entirely hard surfaced at the front. It is considered that the level of landscaping to the front of the site is appropriate for a development within an area with a tight urban grain and enhances the Conservation Area.
40. The Landscape and Tree Officer has recommended that adequate tree pits will be required so that the trees and hedges to the front of the site will establish and grow.

Further details are recommended by condition 5.

41. It is considered that the proposed landscaping is acceptable and will complement the development and provide some enhancements to the character and appearance of the area.

### **Drainage and Flooding**

42. The application site is in Flood Zone 1 where the risk of flooding from rivers or seas is low and, as such all forms of development, including 'more vulnerable' uses, are acceptable.
43. MDD Local Plan policy CC10 states that all development proposals must ensure surface water arising from the proposed development including taking into account climate change is managed in a sustainable manner and this must be demonstrated through a Surface Water Drainage Strategy. The Highway Engineer raises no objection with regard to drainage or flooding implications subject to recommended condition 14.

### **Affordable Housing**

44. The threshold for affordable housing is 5 dwellings or more or residential sites of 0.16 hectares or larger. The site exceeds this threshold and there is a requirement for the provision of 20% of the total number of units (net) to be provided as affordable housing, this equates to 1.8 units. The Affordable Housing Team recommended that the provision of 2 no. 2 bedroom flats would have been appropriate in this case subject to viability.
45. The applicant has submitted a Viability Appraisal with the application demonstrating that in this instance it is not viable for affordable housing to be delivered. The Viability Appraisal has been independently assessed and it has been advised that it is not viable for an Affordable Housing contribution in this instance. The factors that contribute to this conclusion are the relatively high value of the existing land and buildings and the requirement for demolition and contamination remediation works.
46. Paragraph 173 of the NPPF states:
- 'Pursuing sustainable development requires careful attention to viability and costs in plan-making and decision-taking. Plans should be deliverable. Therefore, the sites and the scale of development identified in the plan should not be subject to such a scale of obligations and policy burdens that their ability to be developed viably is threatened. To ensure viability, the costs of any requirements likely to be applied to development, such as requirements for affordable housing, standards, infrastructure contributions or other requirements should, when taking account of the normal cost of development and mitigation, provide competitive returns to a willing land owner and willing developer to enable the development to be deliverable.'*
47. Given the detailed assessment on behalf of the Council, it is not considered to be viable to provide an affordable housing contribution as part of this development. It would therefore be contrary to the NPPF to refuse the application on the grounds of the absence of an affordable housing contribution and therefore no objection is

raised. The Affordable Housing Team therefore raises no objection.

### **Environmental Health**

48. The Environmental Health Team has made the following observation:

*'According to our records the site has been occupied by a vehicle repair/MOT garage and car showroom. We also have evidence to suggest there are 4 No. historic underground petroleum storage tanks on the site. These land uses have the potential to give rise to contamination on the application site and this may present a risk to the proposed end users. As the proposed residential use is sensitive, in accordance with the National Planning Policy Framework 2012 and the principles of sustainable development, the applicant is required to carry out a contamination risk assessment of the site followed by remediation and validation works if found to be necessary.'*

49. A report on intrusive investigation carried out at the site has been submitted with the application. The Environmental Health Team has considered this information and recommended that further information needs to be submitted (recommended condition 17).

50. The site is in close proximity to other residential dwellings. Conditions 15 and 16 to minimise noise and disturbance to the neighbouring residents as a result of construction works are therefore recommended.

### **Biodiversity**

51. The Council's Ecologist has raised no objection to the development. An Ecology Survey has been submitted and this shows that no protected species are using the site. The Ecologist has considered this report and agrees with the survey's findings. The applicant's Ecologist has recommended that bat and bird boxes are accommodated on the site. Condition 22 is recommended to ensure this is carried out.

### **Archaeology**

52. An Archaeological Desk-based Assessment was submitted in support of the application as the proposal site is located within a Conservation Area and an Area of High Archaeological Potential. Berkshire Archelogy has considered the Assessment and agree with the conclusions reached. They raise no objection subject to the recommended condition 21.

### **CIL**

53. As the proposal is for new residential floor space, it would be a CIL liable development. CIL is charged at a rate of £365 per square metre.

## **4.0 CONCLUSION**

The application site is within a Modest Development Location close to local facilities and service and it is considered that the principle of residential development is acceptable in such areas. The design of the scheme will enhance the character and appearance of the Conservation Area. The development will have an acceptable impact on the amenity

of the occupiers of the neighbouring dwellings. It is considered that sufficient parking will be accommodated into the scheme and that no detrimental highway safety implications will arise. Sufficient areas of soft landscaping and adequate garden sizes would be incorporated. The proposed development is considered to be acceptable in all aspects and compliant with the relevant national and development plan policies. The application is accordingly recommended for approval subject to the recommended conditions.

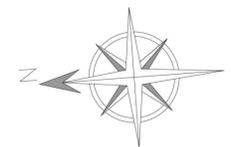
<b>CONTACT DETAILS</b>		
<b>Service</b>	<b>Telephone</b>	<b>Email</b>
Development Management and Regulatory Services	0118 974 6428 / 6429	development.control@wokingham.gov.uk



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**KEY**

-  PROPOSED TREES
-  EXISTING TREES TO BE RETAINED
-  TREES TO BE REMOVED
-  PROPOSED INDICATIVE PLANTING
-  EXISTING PLANTING
-  ±00.00 EXISTING SPOT SITE LEVELS
-  ±00.00 PROPOSED SPOT SITE LEVELS
-  FFL 00.00 PROPOSED HOUSE FINISHED FLOOR LEVELS (SUBJECT TO CONFIRMATION AT WORKING DRAWINGS STAGE)
-  DASHED LINE INDICATES BUILDING TO BE DEMOLISHED
-  BANK
-  A SECTIONLINE



Rev.	Date	Detail
Rev.	Date	Detail

**PLANNING APPLICATION**



**ASCOT DESIGN**  
Timeless architecture

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Client  
**ORACLEMARKER**

Project Title  
**WARGRAVE MOTORS, 15-27 HIGH STREET, WARGRAVE, READING, RG10 8BU**

DRAWING Title  
**PROPOSED COLOURED SITE PLAN**

Scale:	1:200 @ A1	Date:	APR '17	Drawn:	DQ
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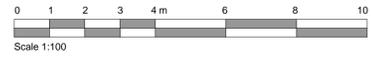
PROPOSED FRONT ELEVATION



PROPOSED SIDE ELEVATION Plots 6 and 8



PROPOSED FLANK ELEVATION Plot 1



PROPOSED REAR ELEVATION  
plot 1

plot 2

plot 3

plot 4

plot 5

plot 6

Rev.	Date	Detail
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**PLANNING APPLICATION**



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Client

**ORACLEMARKER**

Project Title

**WARGRAVE MOTORS,  
15-27 HIGH STREET**

Drawing Title

**PLOTS 1-8  
ELEVATIONS**

Scale:	Date	Drawn
1:100@A1	APR '17	KMB

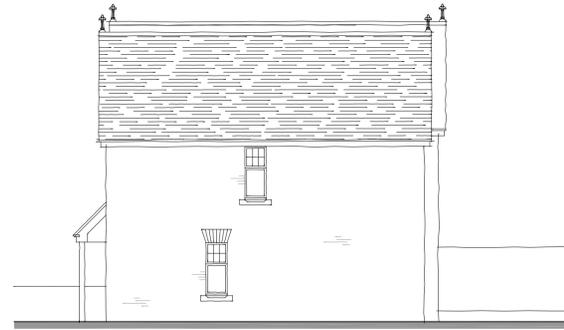
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16-P1382 -202	-

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FRONT ELEVATION



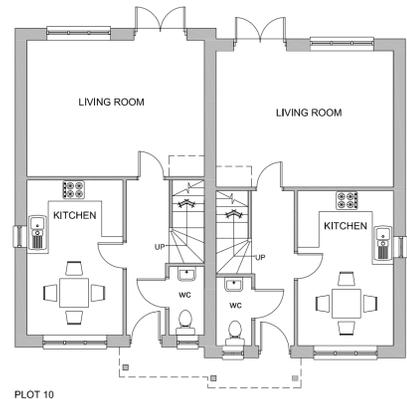
FLANK ELEVATION



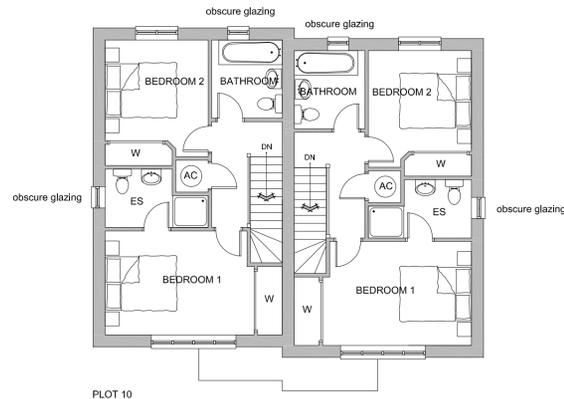
REAR ELEVATION



SIDE ELEVATION



GROUND FLOOR PLAN



FIRST FLOOR PLAN



Scale 1:100

Rev.	Date	Detail

Status

**PLANNING APPLICATION**

**ASCOT DESIGN**  
Timeless architecture

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Client

**ORACLEMARKER**

Project Title

**WARGRAVE MOTORS,  
15-27 HIGH STREET**

Drawing Title

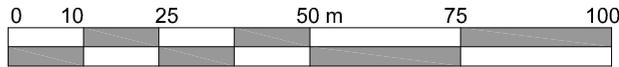
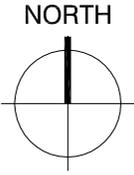
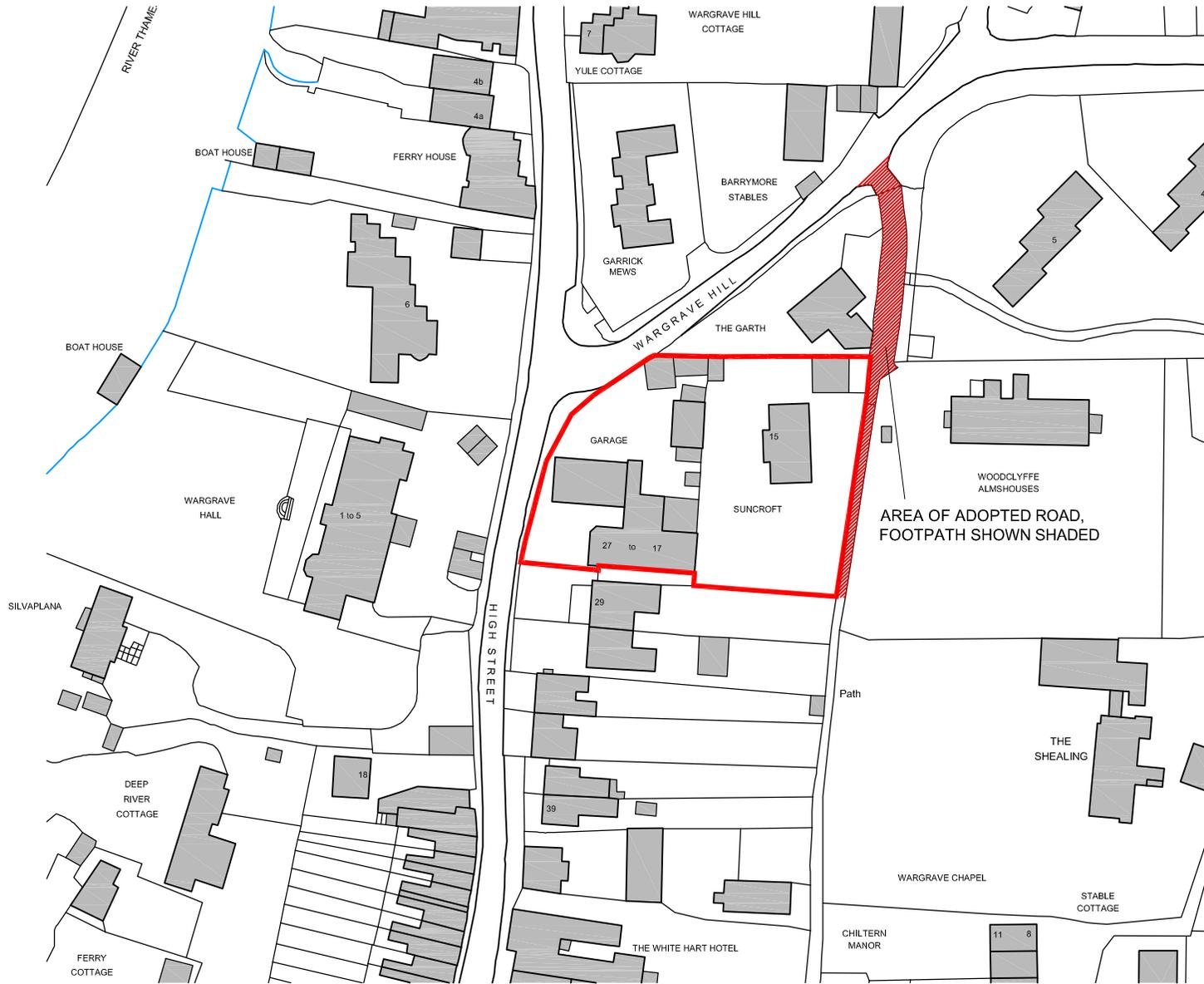
**PLOTS 9-10  
FLOOR PLANS**

Scale	Date	Drawn
1:100@A1	APR '17	KMB

Drawing No.	Rev.
<b>16-P1382 -203</b>	

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Scale 1:1250



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Timeless architecture

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Status	<b>PLANNING APPLICATION</b>
Client	<b>ORACLEMARKER</b>
Project Title	<b>Wargrave Motors, 15-27 High Street, Wargrave, Reading, RG10 8BU</b>

Rev.	Date	Detail
DRAWING Title <b>LOCATION PLAN</b>		
Scale	Date	Drawn
1:1250	APR '17	DQ
DRAWING No. <b>16-P1382-LP</b>		Rev. -

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# Agenda Item 21.

<b>Development Management Ref No</b> 171436	<b>No weeks on day of committee</b> 9/8	<b>Parish</b> Shinfield	<b>Ward</b> Shinfield North	<b>Listed by:</b> WHL application
--	--	----------------------------	--------------------------------	--------------------------------------

<b>Applicant</b>	Wokingham Housing Limited		
<b>Location</b>	Land adjacent to Anson Crescent, Shinfield	<b>Postcode</b>	RG2 8JU
<b>Proposal</b>	Full planning application for the proposed erection of 2no two bedroom flats and 2no one bedroom flats with associated parking and landscaping. (Part retrospective)		
<b>Type</b>	Full		
<b>PS Category</b>	13		
<b>Officer</b>	Pooja Kumar		

<b>FOR CONSIDERATION BY</b>	Planning Committee on 12 <sup>th</sup> July 2017
<b>REPORT PREPARED BY</b>	Head of Development Management and Regulatory Services

## SUMMARY

The application site is within settlement, located adjacent to the Wokingham Borough Council boundary with Reading Borough Council. The site has relatively little amenity value being a small enclosed area of unused space as well as parking for local residents. The site is surrounded by residential development and therefore the proposal of further residential units is considered to accord with Local Plan policy.

The application is for the erection of 4 residential units with associated parking and amenity space. The application is a resubmission of planning application F/2015/0073 which was before planning committee in April 2015. The scheme before this committee is largely the same; however the red line boundary plan has been enlarged to accommodate an additional vehicle parking space and photovoltaic panels have been added to the proposed building.

It is considered that the proposed amendments to planning permission F/2015/0073 will not have any adverse impact on character of the area; highways and neighbour amenity. Therefore, the scheme is considered to be compliant with the development plan and is recommended for conditional approval.

## PLANNING STATUS

- Major development location
- Public Open Space
- Ancient Woodland consultation zone
- Special Protection Area – 7 km
- Groundwater zone – 3
- Nuclear consultation zone

## RECOMMENDATION

**That the committee authorise the GRANT OF PLANNING PERMISSION subject to the following:**

### A. Conditions and Informatives:

### **Approved Details**

1. This permission is in respect of the submitted application plans and drawings numbered '28008-101; 28008-102; 28008-103; 28008-105; 28008-200; 28008-201; 28088-300; 28008-450; 28008-451 and; Design & Access Statement received by the local planning authority on 8<sup>th</sup> May 2014 and the revised plans on 16<sup>th</sup> May 2017. The development shall be carried out in accordance with the approved details unless otherwise agreed in writing by the local planning authority.  
*Reason: For the avoidance of doubt and to ensure that the development is carried out in accordance with the application form and associated details hereby approved.*

### **Materials**

2. The external surfaces of the building hereby approved shall be constructed in accordance with the 'External Materials Schedule' received by the local planning authority on 9<sup>th</sup> June 2017. Development shall not be carried out other than in accordance with the so-approved details.  
*Reason: To ensure that the external appearance of the building is satisfactory.*  
*Relevant policy: Core Strategy policies CP1 and CP3*

### **Hard & Soft Landscaping**

3. All hard and soft landscape works shall be carried out in accordance with the 'Landscape Management Plan (SHF.1236.001.R.002) and Extended Site Plan with Junction (28008-112) prior to the occupation of any part of the development or in accordance with a timetable approved in writing by the local planning authority. Any trees or plants that, within a period of five years after planting, are removed, die or become seriously damaged or defective, shall be replaced in the next planting season with others of species, size and number as originally approved and permanently retained.  
*Reason: In the interests of visual amenity. Relevant policy: Core Strategy policy CP3 and Managing Development Delivery Local Plan policies CC03 and TB21*

### **Tree Protection**

4.
  - a) The tree protection measures shall be implemented in complete accordance with plan number 28008-450 (the Approved Scheme) for the duration of the development (including, unless otherwise provided by the Approved Scheme) demolition, all site preparation work, tree felling, tree pruning, demolition works, soil moving, temporary access construction and or widening or any other operation involving use of motorised vehicles or construction machinery.
  - b) No development (including any tree felling, tree pruning, demolition works, soil moving, temporary access construction and or widening or any other operation involving use of motorised vehicles or construction machinery) shall commence until the local planning authority has been provided (by way of a written notice) with a period of no less than 7 working days to inspect the implementation of the measures identified in the Approved Scheme on-site.
  - c) No excavations for services, storage of materials or machinery, parking of vehicles, deposit or excavation of soil or rubble, lighting of fires or disposal of liquids shall take place within an area designated as being fenced off or otherwise protected in the Approved Scheme.
  - d) The fencing or other works, which are part of the Approved Scheme, shall not be

moved or removed, temporarily or otherwise, until all works including external works have been completed and all equipment, machinery and surplus materials removed from the site, unless the prior approval of the local planning authority has first been sought and obtained.

*Reason: To secure the protection throughout the time that the development is being carried out of trees shrubs or hedges growing within or adjacent to the site which are of amenity value to the area, and to allow for verification by the local planning authority that the necessary measures are in place before development and other works commence Relevant policy: Core Strategy policy CP3 and Managing Development Delivery Local Plan policies CC03 and TB21*

### **Landscape Management**

5. The landscape management shall be carried out as approved with the submitted 'Landscape Management Plan' (Enzygo Environmental Consultants SHF.1236.001.R.002 Dec 2015).

*Reason: In order to ensure that provision is made to allow satisfactory maintenance of the landscaping hereby approved. Relevant policy: Core Strategy policy CP3 and Managing Development Delivery Local Plan policies CC03 and TB21*

### **Contamination**

6. No building shall be occupied until a report detailing the assessment and where necessary mitigation measures undertaken, has been submitted to and approved in writing by the local planning authority.

*Reason: To ensure any contamination on the site is remedied to protect the existing/proposed occupants of the application site and adjacent land. Relevant Policy: Core Strategy Policy CP3*

### **Junction Improvement Works**

7. The junction improvement works must be carried out in full accordance with plan numbers 28008-451; 0100; 0300; 0500; 28008-PD-110; MAP/C3025/SK201; MAP/C3025/SK202 and MAP/C3025/SK203.

*Reason: In the interests of highway safety and convenience. Relevant policy: Core Strategy policies CP3 & CP6.*

### **Access Construction**

8. The access must be constructed in accordance with plan numbers 28008-451; 0100; 0300; 0500; 28008-PD-110; MAP/C3025/SK201; MAP/C3025/SK202 and MAP/C3025/SK203.

*Reason: To ensure adequate access into the site for vehicles, plant and deliveries associated with the development in the interests of highway safety and convenience. Relevant policy: Core Strategy policies CP3 & CP6.*

### **Parking & Turning Space**

9. No part of any building(s) hereby permitted shall be occupied or used until the vehicle parking and turning space has been provided in accordance with the approved plans. The vehicle parking and turning space shall be retained and maintained in accordance with the approved details and the parking space shall remain available for the parking of vehicles at all times and the turning space shall not be used for any other purpose other than vehicle turning.

*Reason: To provide adequate off-street vehicle parking and turning space and to allow vehicles to enter and leave the site in a forward gear in the interests of road safety and*

*convenience and providing a functional, accessible and safe development and in the interests of amenity. Relevant policy: Core Strategy policies CP3 & CP6 and Managing Development Delivery Local Plan policy CC07.*

### **Cycle Parking**

10. No building shall be occupied until secure and covered parking for cycles has been provided in accordance with the approved drawing(s)/details. The cycle parking/storage shall be permanently so-retained for the parking of bicycles and used for no other purpose.

*Reason: In order to ensure that secure weather-proof bicycle parking facilities are provided so as to encourage the use of sustainable modes of travel. Relevant policy: NPPF Section 4 (Sustainable Transport) and Core Strategy policies CP1, CP3 & CP6 and Managing Development Delivery Local Plan policy CC07.*

### **Tree Retention**

11. No trees, shrubs or hedges within the site which are shown as being retained on the approved plans shall be felled, uprooted wilfully damaged or destroyed, cut back in any way or removed without previous written consent of the local planning authority; any trees, shrubs or hedges removed without consent or dying or being severely damaged or becoming seriously diseased within 5 years from the completion of the development hereby permitted shall be replaced with trees, shrubs or hedge plants of similar size and species unless the local planning authority gives written consent to any variation.

*Reason: To secure the protection throughout the time that development is being carried out, of trees, shrubs and hedges growing within the site which are of amenity value to the area. Relevant policy: Core Strategy policy CP3 and Managing Development Delivery Local Plan policies CC03 and TB2 1*

### **Informatives**

1. Thames Water will aim to provide customers with a minimum pressure of 10m head (approx 1 bar) and a flow rate of 9 litres/minute at the point where it leaves Thames Waters pipes. The developer should take account of this minimum pressure in the design of the proposed development.
2. The development hereby permitted is liable to pay the Community Infrastructure Levy. The Liability Notice issued by Wokingham Borough Council will state the current chargeable amount. A revised Liability Notice will be issued if this amount changes. Anyone can formally assume liability to pay, but if no one does so then liability will rest with the landowner. There are certain legal requirements that must be complied with. For instance, whoever will pay the levy must submit an Assumption of Liability form and a Commencement Notice to Wokingham Borough Council prior to commencement of development. For more information see - <http://www.wokingham.gov.uk/planning/developers/cil/cil-processes/>

### **PLANNING HISTORY**

F/2015/0073	Erection of 2no two bed and 2no one bed flats with associated parking and landscaping conditionally approved on 11 <sup>th</sup> June 2016
170870	Application for submission of details to comply with the following conditions of planning consent F/2015/0073 (dated 11/06/2015) – 3. Samples and Materials 4. Hard and Soft Landscaping 5. Tree Protection Plan 6. Landscape Management Plan 7. Harmful Materials Report 8. Junction Improvements approved on 28 <sup>th</sup> June 2017.

<b>SUMMARY INFORMATION</b>	
Site Area	0.05 hectares
Existing units	0
Proposed units	4
Existing use	Area of grassland currently used as an area for rubbish adjacent to an area of parking
Existing parking spaces	5
Proposed parking spaces	9

<b>CONSULTATION RESPONSES</b>	
WBC Drainage	No objection
WBC Ecology	No comments received
WBC Environmental Health	No objection subject to condition (7)
WBC Highways	No objection subject to conditions (8, 9, 10, & 11)
WBC Trees and Landscape	No objection subject to conditions (4, 5, 6 & 12)
WBC Waste Services	No comments received

<b>REPRESENTATIONS</b>
<b>Shinfield Parish Council:</b> No comment
<b>Local Members:</b> No comments received
<b>Neighbours:</b> 1 letter of objection received on the following basis: <ul style="list-style-type: none"> <li>• Unacceptable that the application has been submitted after works have commenced (paragraph 1),</li> <li>• Parking is already unacceptable with the construction and one additional space is inadequate for the proposed development (paragraphs 14 – 16); and</li> <li>• The building could have an impact on light and view to the road (paragraphs 12 – 13).</li> </ul>

<b>APPLICANTS POINTS</b>
<ul style="list-style-type: none"> <li>• Minor changes required to development to resolve site issues and improve scheme</li> <li>• Overall, development provides affordable housing in a sustainable location</li> </ul>

<b>PLANNING POLICY</b>		
National Policy	<b>NPPF</b>	National Planning Policy Framework
Adopted Core Strategy DPD 2010	<b>CP1</b>	Sustainable Development
	<b>CP3</b>	General Principles for Development
	<b>CP4</b>	Infrastructure Requirements
	<b>CP5</b>	Housing mix, density and affordability
	<b>CP6</b>	Managing Travel Demand
	<b>CP7</b>	Biodiversity
	<b>CP9</b>	Scale and Location of Development Proposals
Adopted Managing Development Delivery Local Plan 2014	<b>CC01</b>	Presumption in Favour of Sustainable Development
	<b>CC02</b>	Development Limits
	<b>CC03</b>	Green Infrastructure, Trees and

		Landscaping
	<b>CC04</b>	Sustainable Design and Construction
	<b>CC07</b>	Parking
	<b>CC09</b>	Development and Flood Risk
	<b>CC10</b>	Sustainable Drainage
	<b>TB05</b>	Housing Mix
	<b>TB07</b>	Internal Space Standards
	<b>TB21</b>	Landscape Character
Supplementary Planning Documents (SPD)	<b>BDG</b>	Borough Design Guide – Section 4
	<b>AH</b>	Affordable Housing SPD

## **PLANNING ISSUES**

### **Description of Development:**

1. The scheme is for the erection of a two storey property comprising of two x one bedroom flats and two x two bedroom flats. The building would comprise of two sections with a central stairwell connecting them. The application follows a previously approved scheme, reference F/2015/0073, and proposes alterations to the site layout and the provision of solar panels to the roof. The current application has an enlarged red line which encompasses a larger area of the highway to provide one additional parking space. Other than these two changes however, the proposal and specifically the design of the building has not altered from the approved scheme.
2. The application is on behalf of Wokingham Housing Limited, hence the requirement for determination by the Planning Committee and also why the properties would be provided as affordable housing.

### **Principle of Development:**

3. The National Planning Policy Framework has an underlying presumption in favour of sustainable development which is carried through to the local Development Plan. The Managing Development Delivery Local Plan Policy CC01 states that planning applications that accord with the policies in the Development Plan for Wokingham Borough will be approved without delay, unless material considerations indicate otherwise.
4. Policy CC02 of the MDD sets out the development limits for each settlement as defined on the policies map and therefore replaces the proposals map adopted through the Core Strategy, as per the requirement of policy CP9. Policy CP9 sets out that development proposals located within development limits will be acceptable, having regard to the service provisions associated with the major, modest and limited categories. As the site is within a major development location, the proposal is acceptable in principle.

### **Impact on the Character of the Area:**

5. The site is located adjacent to the Wokingham Borough boundary along Whitley Wood Road whereby the application side of the Road is within WBC and the

opposing side of the Road is within Reading Borough Council. The local area is predominantly residential with the Reading Borough side comprising of two storey semi-detached dwelling with relatively irregular gaps between built form. The WBC side of the street has a more mixed style of dwellings with irregular gaps by virtue of previous backland developments.

6. The site itself is located in a gap between a semi-detached dwelling and a terrace, both of which are two storeys in height. Along the street scene the site is screened by a line of trees. The site comprises mainly of hardstanding to provide parking an access road leading to the same.
7. The proposal introduces a new two storey dwelling into a gap between other two storey dwellings in the street scene. The impact on character has been considered under the previous planning application F/2015/0073 and there have been no policy changes which would require a new assessment under the current application.
8. The proposed materials to be used in the external surfaces of the building have been included within the submitted details which are considered to be acceptable in respect of ensuring that there is no harm to the character and appearance of the site within the street scene. As such, subject to a condition (3) to ensure the proposed development is built in accordance with the approved details, the proposal is considered to accord with policy CP3 of the Core Strategy in this respect.
9. The current application differs from the previous through the introduction of solar panels and an increased area of parking. It is noted that solar panels are not prevalent in the area however these are of a relatively small scale and, due to permitted development regulations, are typical of residential areas across the Borough; as such it is considered that no harmful impact would occur.
10. The increased parking does result in the replacement of a grassed area with hard standing which typically could be detrimental to the street scene. Nonetheless, in this instance, it is noted this part of Anson Crescent was designed with access to parking and as such there is already numerous parking spaces accessed from the highway. In this respect, the partial loss of a grassed area is not considered to alter the character of the area and therefore would not be harmful to the same.
11. Overall the proposed amendments to the approved scheme are considered not to result in any significantly detrimental impacts on the character and appearance of the site and wider street scene. The proposal is considered to accord with policy CP3 of the Core Strategy subject in respect of character.

**Residential Amenities:**

12. As there are no side facing windows at 1 Anson Walk or 9 and 10 Birch Green, it is considered there would be no overbearing or loss of light impact. Similarly, there are no side facing windows in the proposal and therefore no overlooking impact would occur.
13. An objection has been received on the potential loss of light and closing of view towards Whitley Wood Road from Anson Crescent. It is acknowledged that the proposal would infill a gap between two residential buildings; however it is considered that there would be no harmful loss of a view as the view would be to a road. Moreover, the proposed building would be built a distance in excess of 30 metres from dwellings

facing towards Whitley Wood Road. This distance accords with the front-to-front distance advised by the Borough Design Guide in respect of privacy and BRE's Planning for Daylight and Sunlight's distance to maintain adequate light. The proposed development has not been amended from the scheme and therefore the proposed development is considered to accord with policy CP3 in this respect and would not harm residential amenities.

#### **Access and Movement:**

14. **Highway Safety and Road Design:** The proposal would require access from Anson Crescent which is currently quite narrow as the road to the site leads only to parking spaces. As a result, a change to the junction of Anson Crescent is proposed to improve visibility. Subject to a condition, the Highways Officer does not object to this (8 & 9). Additionally, a suitable turning area is also provided.
15. **Parking:** Currently the site contains five parking spaces which are used by local residents. In order to accommodate the development, one of these spaces would be lost however five additional spaces would be provided. These five would be allocated parking for the proposed flats and therefore it is acknowledged there would be a reduction in the overall level of parking for local residents of one space. However, it is considered one parking space could be accommodated on the local highway network without significant detriment to highway safety, particularly given that the street is not immediately adjacent to highway. As such, the Highways Officer has raised no objection subject to a condition to ensure that the development is acceptable (10).
16. **Sustainability:** Secure, covered cycle storage has been indicated and is considered acceptable, subject to a condition (11).

#### **Flooding:**

17. The site is located in Flood Zone 1 is less than a hectare in size. As such there is no requirement to submit a Flood Risk Assessment and there are no known historical flooding issues. Given that the majority of the site is already hard standing it is considered the proposal would not detrimentally impact on flooding and surface water drainage and therefore is acceptable in this respect. Whilst it is acknowledged that the drainage consultant has requested a condition, this relates to detailed drainage matters. From a planning perspective, it is considered a soakaway would be appropriate in this area and therefore further information is not required to determine the application.

#### **Landscape and Trees:**

18. The site itself does not contain any trees and is of little amenity value. However, there is a group of trees to the north west of the site which are adjacent to the road and significantly contribute to the street scene. One of these trees would be removed as a result of the proposal however the other trees would remain.
19. Following the submission of a tree survey as part of the previous application, the Tree and Landscape Officer raised a concern regarding the proximity of the building to a scots pine (Category A tree) located adjacent to the site and the incursion into its root protection area. Furthermore, the Officer raised concerns that the shading of the gardens and the flats themselves due to the proximity to the trees would put undue

pressure on the requirement for works to the trees (i.e. through crown lifting/reduction or the removal of the trees).

20. From a planning perspective, it was previously determined that the potential harmful impact on the trees is outweighed by the benefits of the scheme in providing affordable housing. Whilst the loss of one tree is considered justifiable, the scheme could potentially lead to the loss of further trees. Significant weight has been given to the presence of the trees in the street and therefore any further loss would be harmful to the wider landscape. Previous planning consent required the submission of details of new planting to ensure that there was no further harm to the landscape character of the site and wider street scene. Under the current scheme these details have been re-submitted and it is considered that subject to conditions (4, 5, 6 and 11) for development to be carried out in accordance with the submitted information, the proposal would not have a detrimental impact on the character of the site and wider landscape character along the street scene. As such the proposed development is considered to accord with policies CC03 and TB21 of the MDD Local Plan.

#### **Environmental Health:**

21. No known contamination issues exist on or in close proximity to the site however the site is used for the parking of vehicles and this could potentially lead to some contamination from oil spills. As such, and due to the residential use of the proposal, the Environmental Health Officer requires a condition (7) for a survey to establish if any contamination is present.

#### **Amenity Space for future occupiers:**

22. The Borough Design Guide states that flats should have access to some form of amenity space through the provision of private or communal garden space. The proposal would provide two sections of amenity space, one for plots 1 and 3, the other for plots 2 and 4. Both of these would have an area of approximately 42m<sup>2</sup> which although of a relatively small size, could still accommodate typical garden activities. As such, the proposal is considered acceptable in this respect.

#### **Internal Space Standards:**

23. Internal space standards for new dwellings are set nationally. For a one bedroom flat the minimum requirement is 50m<sup>2</sup> (two bed spaces) and a two bedroom flat 61m<sup>2</sup> (three bed spaces). The proposal would be 50m<sup>2</sup> and 65m<sup>2</sup> respectively and would therefore meet the requirement. As such, the proposal is acceptable in this respect.

#### **Biodiversity:**

24. As no buildings would need to be demolished and the site is only used for dumping rubbish and parking cars, no harmful impact on biodiversity is considered to occur. As such, the Biodiversity Officer raises no objection to the proposal.

#### **Sustainability:**

25. The impact on sustainable design and construction was considered acceptable through application F/2015/0073. One of the alterations of the scheme however includes the provision of solar panels. Policy CC04 does not require a proposal of this scale to

provide such renewable energy and neither does the scheme meet the requirement of policy CC05 in terms of achieving a 10% reduction in carbon emissions. It is noted however that the applicant requires the solar panel provision for aspects outside of the planning system. Given that the solar panels would provide renewable energy, which is a benefit of the scheme, no objection is raised to achieving above and beyond the policy requirement.

**Community Infrastructure Levy, Affordable Housing and SPA:**

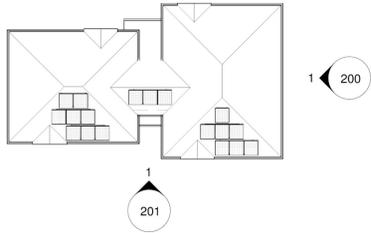
- 26. **Community Infrastructure Levy:** As the proposal is for the construction of new dwellings, it would be a CIL liable development. The CIL charge for new residential development is set at £365 per square metre for any net increase in residential floor space. It is noted however that the scheme would likely be for affordable housing and therefore an exemption for the CIL charge could potentially be applied for.
- 27. **Affordable Housing:** The proposal does not meet the threshold for the provision of affordable housing as per Policy CP5 of the Core Strategy. However, the applicant proposes to provide the properties as affordable housing which exceeds the requirements of the development plan and is therefore beneficial to the scheme.
- 28. **Special Protection Area:** Whilst the proposal is within 7 km of the Thames Basin Heath Special Protection Area, it does not meet the threshold for mitigation against the SPA. As such, a mitigation strategy or contribution to the Council's agreed mitigation strategy is not required.

**CONCLUSION**

The application is considered to be acceptable in terms of the impact on the character of the area, neighbouring properties and highways. Most of the parking that would be lost as part of the proposal would be replaced and the proposal would provide adequate living amenities for future occupiers. Given the current use of the site, the proposal is considered to have a positive impact on the area and, as such, the proposal is recommended for conditional approval.

**CONTACT DETAILS**

Service	Telephone	Email
Development Management and Regulatory Services	0118 974 6428 / 6429	development.control@wokingham.gov.uk



**Material Schedule**

**Roof Finish**

Plain Concrete Roof Tiles: Red

**Facia and Gutter System**

White UPVC Fascia and Soffits  
Black uPVC rainwater goods

**Masonry**

Wienerberger Tuscan Multi Stock

**Soldier Course**

Wienerberger Tuscan Multi Stock

**Windows**

White UPVC

**Dormers**

Marley Eternit Weatherboard Cladding



**1 Side Elevation 1**  
1 : 50



**2 Rear Elevation**  
1 : 50

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No deviation from this drawing will be permitted without prior consent of the architect.  
All drawings are prepared subject to a full measured and structural survey of the buildings and site.  
All structural work is subject to the appointment of a structural engineer to confirm and agree the structural proposals.  
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rev	notes	date	drawn	checked

client



**PLANNING**



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project  
Proposed Development of 4 no flats - Anson Crescent

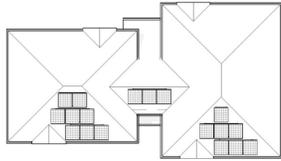
detail  
Elevations - Sheet One

drawn	checked	status
RJN	HM	PLANNING
date 19.04.17		
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2

201  
2



### Material Schedule

#### Roof Finish

Plain Concrete Roof Tiles: Red

#### Facia and Gutter System

White UPVC Fascia and Soffits  
Black uPVC rainwater goods

#### Masonry

Wienerberger Tuscan Multi Stock

#### Soldier Course

Wienerberger Tuscan Multi Stock

#### Windows

White UPVC

#### Dormers

Marley Eternit  
Weatherboard Cladding

**2 Side Elevation 2**  
1 : 50



**1 Front Elevation**  
1 : 50

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**health & safety:**  
exceptional risks involving specific methods of construction or exceptional maintenance issues:

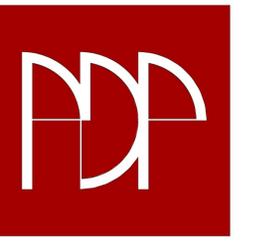
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rev	notes	date	drawn	checked

client



## PLANNING



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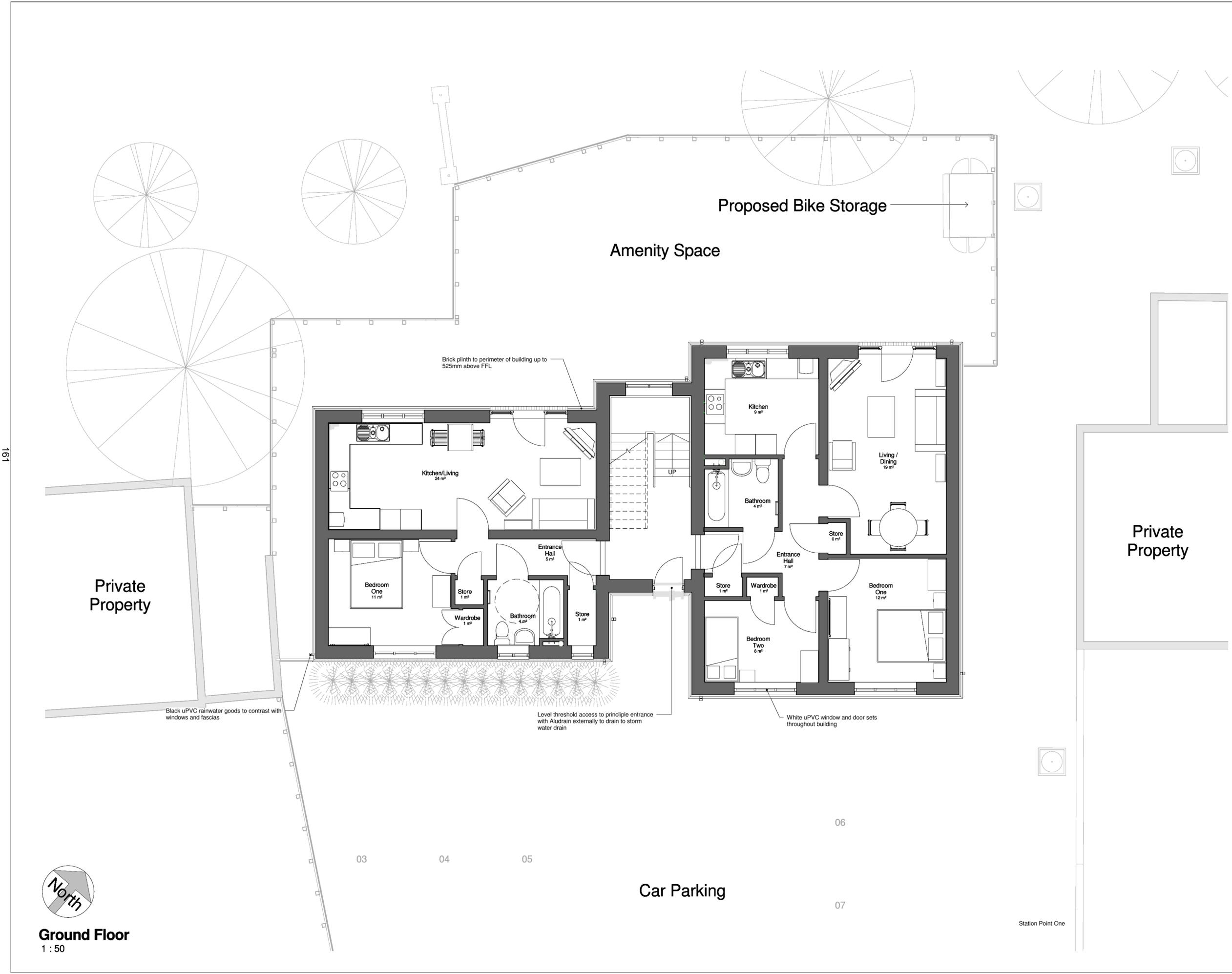
project  
 Proposed Development of 4 no flats - Anson Crescent

detail  
 Elevations - Sheet Two

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All structural work is subject to the appointment of a structural engineer to confirm and agree the structural proposals.

os promap licence no. 100020449.

**health & safety:**  
exceptional risks involving specific methods of construction or exceptional maintenance issues:

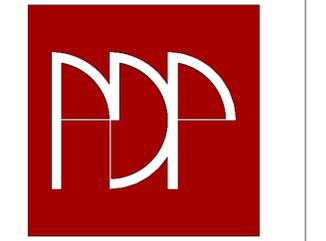
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rev	notes	date	drawn	checked

client

**Wokingham Housing**

**PLANNING**



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project	Proposed Development of 4 no flats - Anson Crescent		
detail	Ground Floor Plan		
drawn	RJN	checked	HM
date	07.04.17	status	PLANNING
a1 scale	1 : 50	a3 scale	28008 - 102

161

Private Property

Private Property

Car Parking

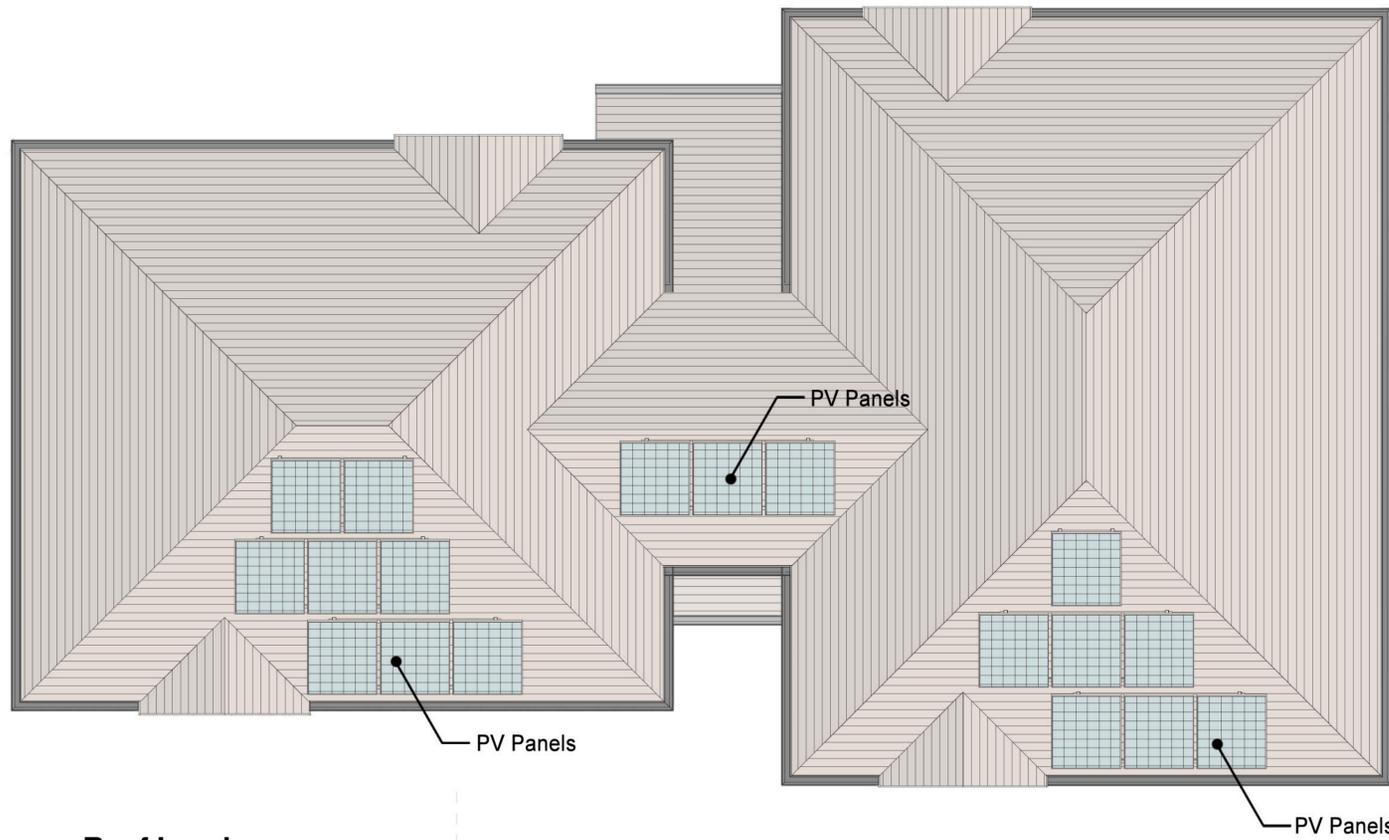
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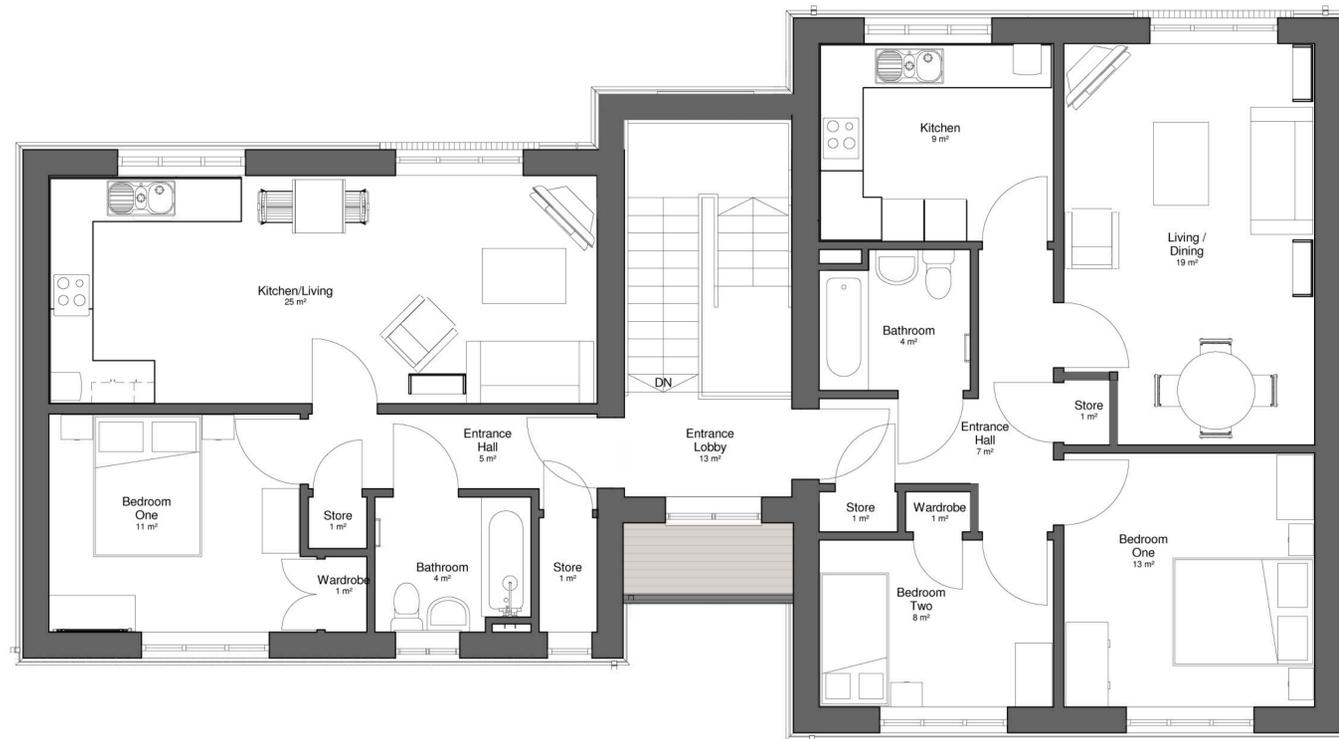
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**1 Roof Level**  
1 : 50



**2 First Floor - FFL**  
1 : 50



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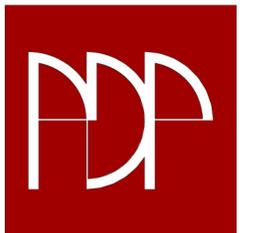
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rev	notes	date	drawn	checked

client



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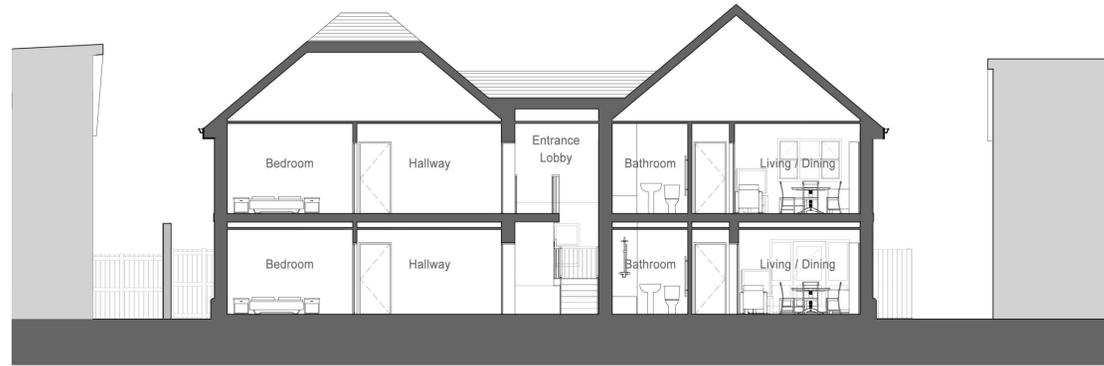
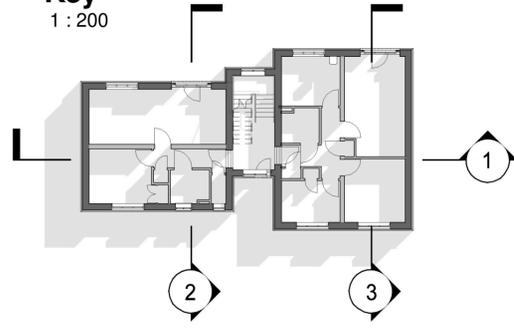
project  
Proposed Development of 4 no flats - Anson Crescent

detail  
First Floor and Roof Plan

drawn	checked
RJN	HM
date	status
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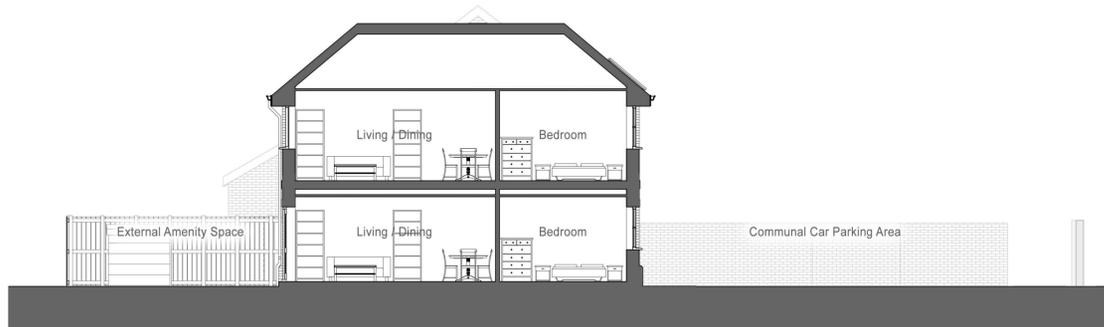
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**1 Section 1**  
1 : 100



**2 Section 2**  
1 : 100



**3 Section 3**  
1 : 100

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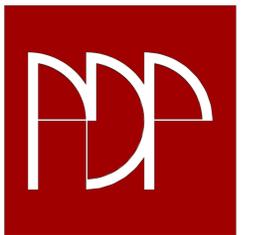
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client



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project  
Proposed Development of 4 no flats - Anson Crescent, Wokingham

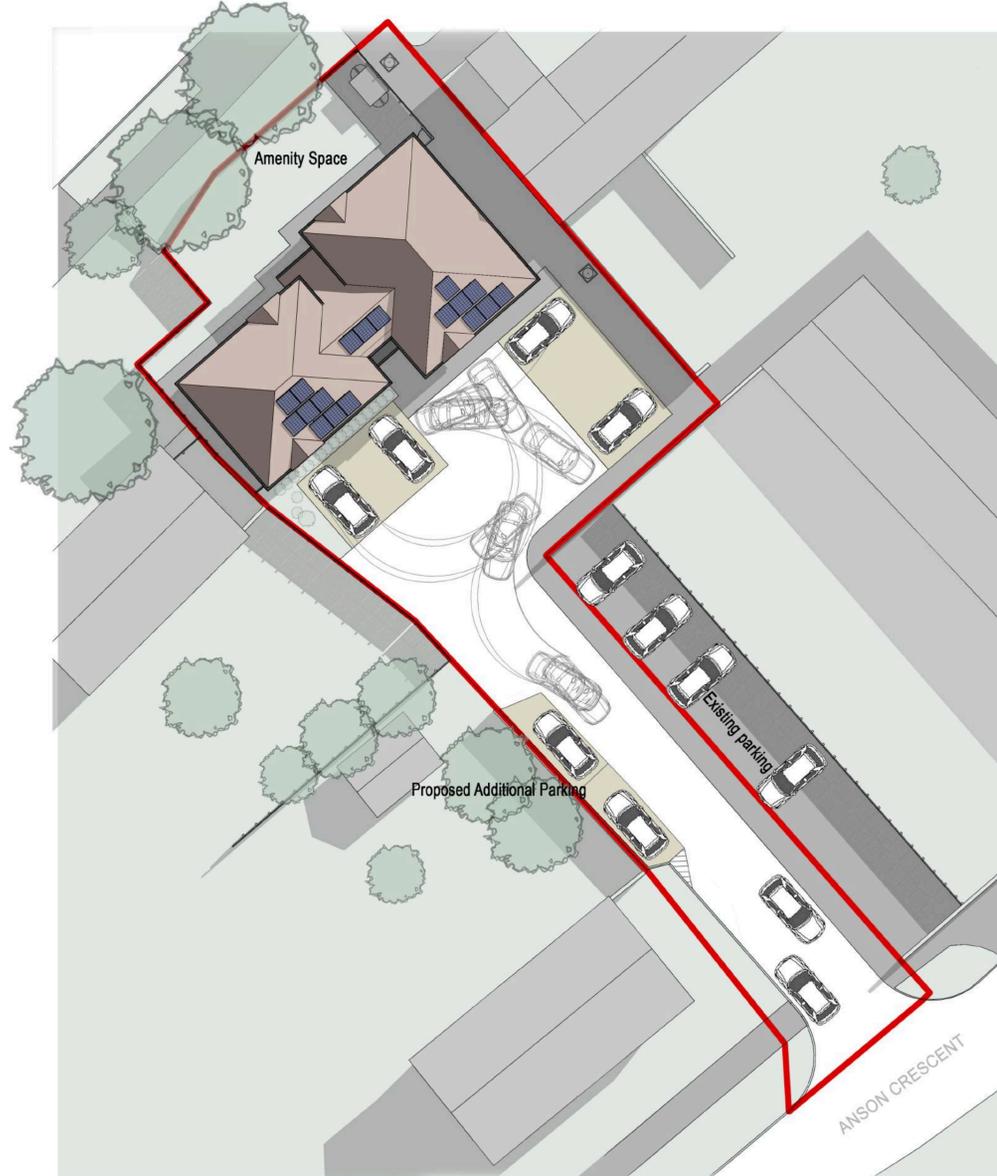
detail  
Proposed Sections

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SJW	HJM
date	status
	PLANNING
a1 scale	a3 scale
As indicated	28008 - 300
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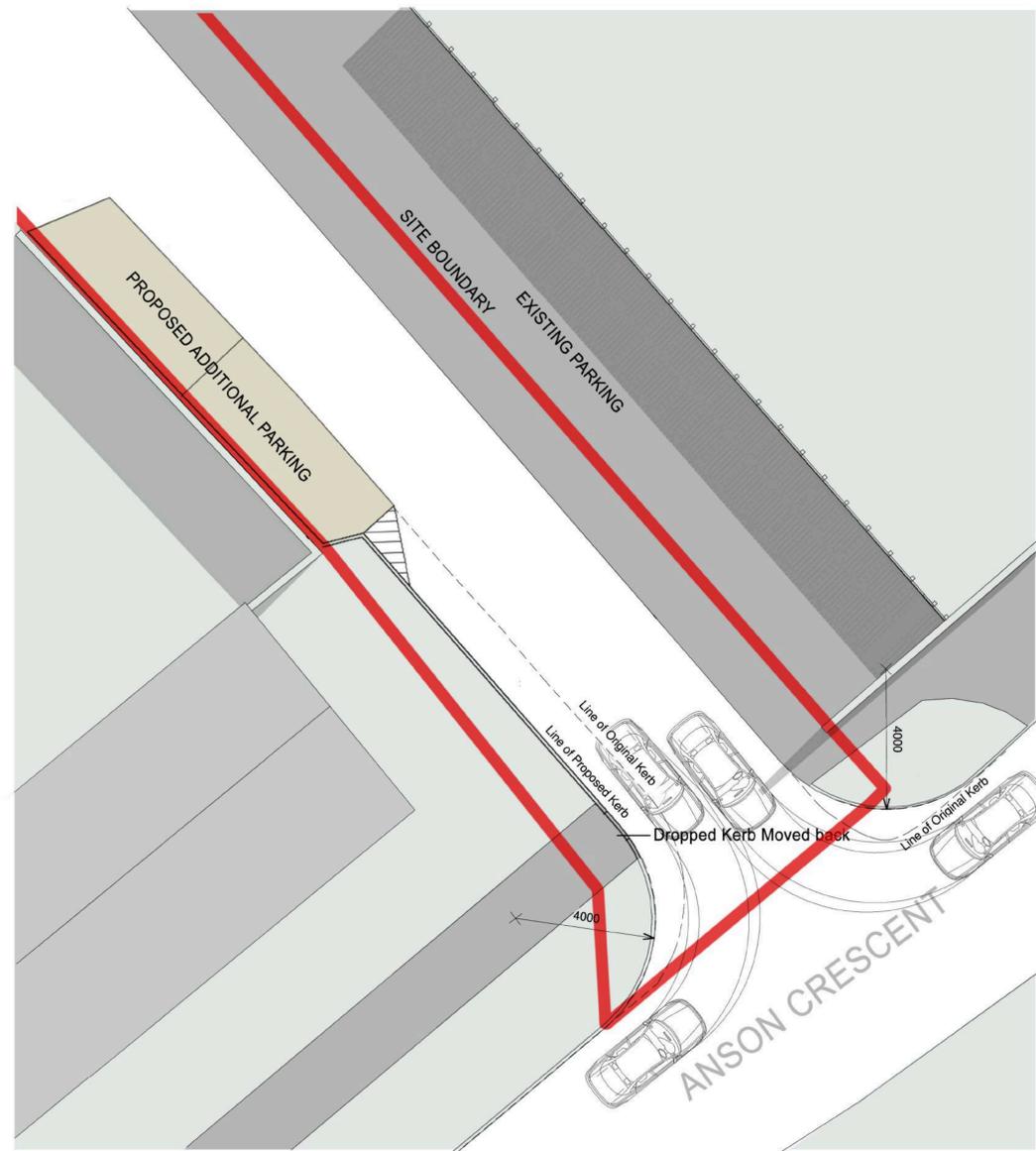
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**Location Plan**  
1 : 1250



**Site Plan**  
1 : 200



**New Junction Improvements**  
1 : 100

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project  
Proposed Development of 4 no flats - Anson Crescent

detail  
Site Plan and New Junction

drawn	SJW	checked	HM
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date	status	PLANNING
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# Agenda Item 22.

<b>Development Management Ref No</b>	<b>No weeks on day of committee</b>	<b>Parish</b>	<b>Ward</b>	<b>Listed by:</b>
170580	17/13	Sonning	Sonning	Major

<b>Applicant</b>	Rugby Football Union
<b>Location</b>	Old Redingensians Sports Ground, Old Bath Road, Sonning, RG4 6TQ
<b>Proposal</b>	Full planning for the proposed installation of artificial grass pitch (AGP) to form a full sized playing enclosure measuring 122 x 80m with associated technical areas. Installation of new pitch perimeter and associated gated entrance to form a playing enclosure, new hardstanding, new (artificial) flood lighting, new maintenance equipment store and retention of soil arising onsite to form a sculpted grass mound to an adjacent grass pitch.
<b>Type</b>	Major Application
<b>PS Category</b>	006
<b>Officer</b>	Pooja Kumar

**FOR CONSIDERATION BY** Planning Committee on 12<sup>th</sup> July 2017  
**REPORT PREPARED BY** Head of Development Management and Regulatory Services

## **SUMMARY**

The application site is an established Sports Ground used for rugby training, matches and hosting events. The site is located within designated countryside and in close proximity to the settlements of Woodley, Sonning and Charvil. The site has a verdant character and visually contributes to the gap between these settlements.

The application seeks consent for the installation of an artificial grass pitch with hardstanding, floodlighting, equipment storage and pitch gates and boundary to improve the sporting facilities on site. Vehicular parking on site would be increased to accommodate additional users. The proposal does not seek to increase the opening hours of the pitches but would allow use of the site throughout the day and all year round.

The NPPF supports development of sports facilities and Core Strategy Policy CP11 encourages development within the countryside which promotes recreation outside of settlement limits. In respect of character and appearance the proposal would result in limited alteration of the site within the street scene due to the screening of the site along the A4 Bath Road. With regards to other aspects, parking would be in accordance with the Borough's standards and no harmful impact would occur on residential amenity. The scheme is considered to be compliant with the development plan subject to conditions and is recommended for conditional approval.

## **PLANNING STATUS**

- Countryside
- Sand & Gravel extraction
- Archaeological Interest
- Green Route

- Groundwater Protection Zone
- Mineral Site Consultation Zone

## RECOMMENDATION

**That the committee authorise the GRANT OF PLANNING PERMISSION subject to the following:**

### **A. Conditions and informatives:**

#### **Timescale**

1. The development hereby permitted shall be begun before the expiration of three years from the date of this permission.

*Reason: In pursuance of s.91 of the Town and Country Planning Act 1990 (as amended by s.51 of the Planning and Compulsory Purchase Act 2004).*

#### **Plans**

2. This permission is in respect of the submitted application plans and drawings numbered 03; 04; 06; 07; Optivision Breakthrough Downlight Specification; rugby Activity Assessment 1, 2 and 3 and; photographs of proposed surfaces and lighting; plan number 05 Revision 01; Design, Access and Planning Statement; and Artificial Grass Pitch Lighting Details (CalcuLux Area 7.7.1.0) received by the local planning authority on 25/05/2017; plan numbered 09 Revision 01 and BRS17-71 Geo Report received by the local planning authority on 01/06/2017 and plans 01 Rev 01 and 02 Rev 02 received by the local planning authority on 30. The development shall be carried out in accordance with the approved details unless otherwise agreed in writing by the local planning authority.

*Reason: For the avoidance of doubt and to ensure that the development is carried out in accordance with the application form and associated details hereby approved.*

#### **External materials**

3. Except where stated otherwise on the approved drawings, the materials to be used in the construction of the proposed pitch hereby permitted shall be of similar appearance to those used in the existing pitch, unless otherwise agreed in writing by the local planning authority.

*Reason: To ensure that the appearance of the pitch is satisfactory and in accordance with sports guidance. Relevant policy: Core Strategy policies CP1 and CP3*

#### **Earth Mounding and Contouring**

4. Prior to the commencement of the development, details of earthworks shall be submitted to and approved in writing by the local planning authority. These details shall include the proposed grading and mounding of land areas including the levels and contours to be formed, showing the relationship of proposed mounding to existing vegetation and surrounding landform. The Earthworks shall be carried out in accordance with the approved details and permanently so retained.

*Reason: In the interests of the amenity and landscape character of the area. Relevant policy: Core Strategy policy CP3 and Managing Development Delivery Local Plan policies CC03 and TB21*

#### **Landscaping**

5. Prior to the commencement of the development there shall be submitted to and approved in writing by the local planning authority a scheme of landscaping, which

shall specify species, planting sizes, spacing and numbers of trees/shrubs to be planted, and any existing trees or shrubs to be retained.

Planting shall be carried out in accordance with the approved details in the first planting and seeding seasons following the occupation of the building(s).

Any trees or plants which, within a period of 5 years from the date of the planting (or within a period of 5 years of the occupation of the buildings in the case of retained trees and shrubs) die, are removed or become seriously damaged or diseased shall be replaced in the next planting season with others of similar size and species or otherwise as approved in writing by the local planning authority.

*Reason: To ensure adequate planting in the interests of visual amenity. Relevant policy: Core Strategy policy CP3 and Managing Development Delivery Local Plan policies CC03 and TB21 (and TB06 for garden development)*

### **Construction Hours**

6. No work relating to the development hereby approved, including works of demolition or preparation prior to building operations, shall take place other than between the hours of 8:00 am and 6:00 pm Monday to Friday and 8:00 a.m. to 1.00p.m. Saturdays and at no time on Sundays, Bank or National Holidays.

*Reason: In the interests of the amenities of neighbouring occupiers*

### **Dust**

7. Before development commences the applicants shall submit for written approval to the Local Planning Authority a scheme of works that sets out the measures that will be taken to minimise dust arising from the development. The dust mitigation measures identified in the scheme shall be carried out and maintained until construction is complete.

*Reason: To protect the amenity of local residents during the construction period*

### **Deliveries**

8. No deliveries of equipment and/or materials shall be accepted outside the hours of 8:00 am and 6:00 pm Monday to Friday and 8:00 am to 1.00 pm Saturday and at no time on Sundays, Bank or National Holidays.

*Reason: In the interests of the amenities of neighbouring occupiers*

### **Drainage**

9. No development shall take place until full details of the drainage system for the site have been submitted to and approved in writing by the LPA. The details shall include:

- Results of intrusive ground investigation demonstrating seasonal high groundwater levels for the site and infiltration rates in accordance with BRE365.
- Demonstration that the base of SuDS features are at least 1m above seasonal groundwater level.
- Full calculations demonstrating the performance of soakaways or capacity of attenuation features to cater for 1 in 100 year flood event with a 40% allowance for climate change and runoff controlled at Greenfield rates, or better.
- Calculations demonstrating that there will be no flooding of pipes for events up to and including the 1 in 100 year flood event with a 40%

allowance for climate change.

- A drainage strategy plan for the proposed development, including pipe details with invert levels.
- A maintenance management plan for the SuDS features throughout the lifetime of the development, as well as who will be responsible for the maintenance.

*Reason: This is to prevent increased flood risk from surface water run-off.*

*Relevant policy: NPPF Section 10 (Meeting the Challenge of Climate Change, Flooding and Coastal Change), Core Strategy policy CP1 and Managing Development Delivery Local Plan policies CC09 and CC10*

### **Construction Method Statement**

10. No development shall take place, including any works of demolition, until an updated Construction Method Statement has been submitted to, and approved in writing by, the local planning authority. The approved Statement shall be adhered to throughout the construction period. The Statement shall provide for:

- i) the parking of vehicles of site operatives and visitors,
- ii) loading and unloading of plant and materials,
- iii) storage of plant and materials used in constructing the development,
- iv) the erection and maintenance of security hoarding including decorative displays and facilities for public viewing, where appropriate,
- v) wheel washing facilities,
- vi) measures to control the emission of dust and dirt during construction,
- vii) a scheme for recycling/disposing of waste resulting from demolition and construction works.

*Reason: In the interests of highway safety & convenience and neighbour amenities.*

*Relevant policy: Core Strategy policies CP3 & CP6.*

### **Hours of Operation**

11. The artificial grass pitch and external lighting hereby approved shall only be used during the hours of 9am and 10pm Monday to Sunday including Public and Bank Holidays and at no other times without the prior written consent of the Local Planning Authority.

*Reason: To safeguard residential amenities. Relevant policy: Core Strategy policies CP1 and CP3 and Managing Development Delivery Local Plan policies CC06 and TB20.*

### **External Lighting**

12. Prior to commencement of development there shall be submitted to and approved in writing by the Local Planning Authority details of the proposed external lighting, including details of baffles and/or louvres to minimise lightspill.

*Reason: To safeguard residential amenities. Relevant policy: Core Strategy policies CP1 and CP3 and Managing Development Delivery Local Plan policies CC06 and TB20.*

### **Transport**

13. The pitch hereby approved shall not be used until the vehicle parking space has been provided in accordance with the approved plans. The vehicle parking space shall be permanently maintained and remain available for the parking of vehicles at all times.

*Reason: To ensure adequate on-site parking provision in the interests of highway safety, convenience and amenity. Relevant policy: Core Strategy policies CP3 &*

14. Prior to operation of the artificial grass pitch, an event car parking management plan shall be submitted to and approved in writing by the local planning authority.  
*Reason: In the interest of highways safety and parking. Relevant policy Core strategy CP6*

### **Archaeology**

15. No development shall take place within the site until the applicant, or their agents or their successors in title, has secured the implementation of a programme of archaeological work (which may comprise more than one phase of work) in accordance with a written scheme of investigation, which has been submitted to and approved in writing by the Local Planning Authority. The development shall only take place in accordance with the detailed scheme approved pursuant to this condition.  
*Reason: To ensure that any archaeological remains within the site are adequately investigated and recorded or preserved in situ in the interest of protecting the archaeological heritage of the borough.*

### **Informatives**

1. The applicant is advised that the pitch should be built in accordance with RFU guidance note 7: Artificial Rugby Turf and tested bi-annually by an accredited testing laboratory in order to achieve and maintain World Rugby Regulation 22.
2. The applicant is reminded that this approval is granted subject to conditions which must be complied with prior to the development starting on site. Commencement of the development without complying with the pre-commencement requirements may be outside the terms of this permission and liable to enforcement action. The information required should be formally submitted to the Council for consideration with the relevant fee. Once the details have been approved in writing the development should be carried out only in accordance with those details. If this is not clear please contact the case officer to discuss.
3. The Local Planning Authority has acted positively and proactively in determining this application by assessing the proposal against all material considerations, including planning policies and any representations that may have been received. This planning application has been the subject of positive and proactive discussions with the applicant in terms of:
  - addressing the evolving planning policy context;
  - addressing concerns relating to highway safety;
  - extending the determination period of the application to allow for a geophysical survey to be undertaken and submitted for consideration;
  - amended plans being submitted by the applicant to overcome concerns relating to highways;The decision to grant planning permission in accordance with the presumption in favour of sustainable development as set out in the NPPF is considered to be a positive outcome of these discussions.

**RELEVANT PLANNING HISTORY**

No relevant planning history

**SUMMARY INFORMATION**

Site Area	1.06ha
Existing car parking	263
Proposed car parking	313
Existing & Proposed Coach Parking	1
Existing & Proposed Minibus Parking	2

**CONSULTATION RESPONSES**

WBC Highways	No objection subject to conditions (10, 13 & 14)
WBC Drainage	No objection subject to condition (9)
WBC Trees & Landscape	No objection subject to condition (5)
WBC Biodiversity	No objection
WBC Environmental Health	No objection subject to conditions (6, 7, 8, 11 & 12)
Berkshire Archaeology	No objection subject to condition (15)
Sport England	Support the application subject to condition (3) and informative (1)

**LOCAL REPRESENTATIONS**

A local resident has *commented* on the following:

- Please ensure that there is no direct light from the floodlighting into the adjacent residential care home (paragraph 27); and,
- Precautions required in respect of Drainage on site (paragraph 28).

Sonning & Sonning Eye Society *Object* due to the following:

- Increased volume of traffic using the access road (paragraphs 20-24); and,
- Impact of lighting on the neighbouring residential care home (paragraph 27).

**Sonning Parish Comments:**

Object to the application on the basis of

- Fuller public consultation should be undertaken (paragraph 36),
- Impact on highways and parking in the local area (paragraphs 20-24),
- Concerns over flood lighting and condition recommended for hours of lighting (paragraph 27 and condition 12).

**PLANNING POLICY**

National Policy	NPPF	National Planning Policy Framework
Adopted Core Strategy DPD 2010	CP1	Sustainable Development
	CP2	Inclusive Communities
	CP3	General Principles of Development
	CP6	Managing Travel Demand
	CP9	Scale and location of development

		proposals
	CP11	Proposals outside Development Limits (Countryside)
Adopted Managing Development Delivery Local Plan 2014	CC01	Presumption in Favour of Sustainable Development
	CC02	Development Limits
	CC03	Green Infrastructure, Trees and Landscaping
	CC06	Noise
	CC07	Parking
	CC09	Development and Flood Risk
	CC10	Sustainable Drainage
	TB21	Landscape Character
	TB25	Archaeology
Supplementary Planning Documents (SPD)	BDG	Borough Design Guide

## PLANNING ISSUES

### Description of Development:

1. The scheme seeks approval for the installation of an artificial grass pitch (AGP) to form a full sized playing enclosure, measuring 122m x 80m with associated equipment and facilities on site to improve the quality of playing field. The below table outlines all of the pitch arrangements which the proposed artificial grass pitch could accommodate:

Activity	Type	Pitch Size	Quantity
Rugby Union	15v15		1
Playing Enclosure		122 x 80m	
Playing Area		112 x 70m	
Field of Play		100 x 70m	
In Goals		70 x 6m (2no.)	
Perimeter Area		5m to all sides	
Training Areas	Various	35 x 25m	8

2. The proposal includes a pitch barrier and gates which would be square posts with oval rail fence with finished white in colour. The barrier and gates would be no more than 1.2 metres in height.
3. To all four sides of the proposed AGP would be hardstanding in the form of porous asphalt surfacing in grey/black. This is for spectators and emergency access vehicles to have a flat and clean surface. The hardstanding would measure 124m x 2.5-4m along the northern side of the pitch; 124m x 1.5m to the southern side of the pitch and; 80m x 1.5m to the east and west sides of the pitch.
4. A maintenance equipment storage container would be installed to the south of the proposed pitch. The container would be steel and finished dark green in colour with

steel ramps for easy equipment access and egress. The container would measure 12.12m (l) x 2.44m (w) x 2.89m (h) and would have an opening to one side only.

5. As a result of the proposed development there would be removal of soil which would be re-used on site to create a grassed mound to the north east corner of the site. The sculpted mound would be 100m x 5m in its length and width. Further details on the mound, including its proposed height are requested by condition 4.
6. Officers have secured a reduction to the height of the proposed floodlighting to be 15 metres, which is the same as the height of existing light columns. The light columns would be arranged with 2 columns on the northern side of the pitch and 2 lighting columns on the southern side. The lighting pillars would have 16 luminaries which would face the proposed pitch.

### **Principle of development**

7. The National Planning Policy Framework (NPPF) has an underlying presumption in favour of sustainable development which is carried through to the local Development Plan. The Managing Development Delivery Local Plan Policy CC01 states that planning applications that accord with the policies in the Development Plan for Wokingham Borough will be approved without delay, unless material considerations indicate otherwise.
8. Section 8 of the NPPF relates to 'promoting healthy communities' and paragraph 73 states that 'access to high quality open spaces and opportunities for sport and recreation can make an important contribution to the health and well-being of communities'. The NPPF attaches great weight to the retention of playing fields and advises that only under certain criteria should be built upon.
9. The application site is located outside of settlement limits and within designated countryside whereby policy CP11 of the Core Strategy would apply. The policy seeks to protect the intrinsic beauty of countryside locations. The policy advises that; 'In order to protect the separate identity of settlements and maintain the quality of the environment, proposals outside of development limits will not normally be permitted except where:
  - 1) It contributes to diverse and sustainable rural enterprises within the Borough, or in the case of other countryside based enterprises and activities, it contributes and/or promotes recreation in, and enjoyment of, the countryside; and
  - 2) It does not lead to excessive encroachment or expansion of development away from the original buildings; and
  - 3) It is contained within suitably located buildings which are appropriate for conversion, or in the case of replacement buildings would bring about environmental improvements; or
  - 4) In the case of residential extensions, does not result in inappropriate increases in the scale, form or footprint of the original building;
  - 5) In the case of replacement dwellings the proposal must:
    - i) Bring about environmental improvements
    - ii) Not result in inappropriate increases in the scale, form or footprint of the original building
  - 6) Essential community facilities cannot be accommodated within development limits or through the re-use/replacement of an existing building;
  - 7) Affordable housing on rural exception sites in line with CP9.

10. Criteria 1 and 2 are most relevant to the proposed development as it relates to the improvement of an existing sports facility within the designated countryside. The proposed works would replace a grassed pitch with an artificial grassed pitch which would allow for more intensive use of the site and allow all year round use by sports and communities groups. The proposal would contribute to a diverse rural enterprise and promote recreation outside of settlement limits. Furthermore, although it is recognised that there would be some introduction of hardstanding around the proposed pitch's periphery, this would be limited in scale and would be ancillary to the use.

**Character of the Area:**

11. The application site is an established Rugby club located to the East of Reading along the A4 Bath Road. The site adjoins a residential care home to the west, the A4 Bath Road to the north, Sonning Golf Club to the south and barns in office use (Model Farm) to the east. Beyond the application site, when travelling to the east along the A4 Bath Road the local area comprises of open fields before Charvil.

12. The site is relatively flat, the level of the land is below the land level of the A4 Bath Road and the boundary treatment around the site consists of mature landscaping on most sides with post and rail fencing. The site itself comprises of a club house; grassed pitches for playing and training and; hardstanding for parking and pedestrian access around the site.

13. Visually the application site forms part of the open countryside, and it is considered to enhance the verdant character of the area by virtue of the mature boundary treatment along the A4. The proposed development comprises of mainly groundworks to install an artificial playing pitch to allow a greater use of the site. The alteration to the ground surface and addition of hardstanding would not be visually prominent from outside of the site and therefore is considered to result in no detriment to the open, verdant character and appearance of the site and wider countryside. The installation of post and rail fencing and the gates are considered to have a limited visual alteration to the appearance of the site from public view points. There is existing white post and rail fencing within the site around the pitch periphery and the proposed development would not significantly alter the existing visual appearance of the site from public vantage points outside the site.

14. To the southern end of the site a container would be installed with a steel ramp for storage purposes. The container would be coloured dark green and would be located over 110 metres from the site's access gate. It is considered that the container would not have a detrimental impact on the character and appearance on site; the container has a function use ancillary to the rugby pitch and is located at the furthest point away from the street scene. It is therefore considered that the proposed container would not harmfully alter the character and appearance of the site and its wider setting.

15. There is considered to be no harm from the proposed lighting which would be of a slim profile and 15 metres in height. The lighting is not considered to be any different from floodlighting that is ubiquitous at the application site and other sports site's in Sonning such as the Reading Cricket Club. Furthermore the site is on a lower land level than the A4 Bath Road, which, combined with the mature tree

screening to the north elevation is considered to sufficiently screen the lighting columns.

16. During evening hours it is acknowledged that the proposed lighting would be more prominent within the local landscape; however the site is an established sports ground with floodlighting on site for the training area and, as such, flood lighting is a common feature of the site and wider area and would not appear out of context for this location. Subject to a condition restricting the hours of use of the lighting, and further details to be submitted for the use of baffles and/or shields to ensure that spill is minimised, it is considered that illumination in the evenings would not have a significantly detrimental impact on the character and appearance of the local area.
17. Overall it is considered that the proposed development would not have a detrimental impact on the character and appearance of the site and wider countryside. The proposal would maintain the verdant character of the site in the wider street scene and is considered not to result in encroachment within designated countryside. As such the proposal is considered to accord with policies CP3 and CP11 of the Core Strategy.

#### **Trees & Landscape:**

18. WBC Trees and Landscape officers have raised no objection to the proposed development. The proposed development would not result in the loss of any existing trees and shrubs on site, and is considered by officers to maintain the existing landscape character of the site.
19. The reduction in height of the columns to 15 metre high columns is welcomed by trees and landscape officers and it is advised that a condition to enhance boundary treatment in the form of native hedge planting and hedgerow should be attached to planning permission (5), subject to which the proposal is considered to accord with policies CC03 and TB21 of the MDD Local Plan.

#### **Highways:**

20. The proposed development would result in an increase in the use of the site during daytime hours; however WBC Highways officer have advised that the proposed number of people training and number of spectators are not significant enough to have a detrimental impact on the local highway network.
21. The application site provides 263 vehicle parking spaces on site which is in accordance with the Borough's adopted standards. As part of the proposal and additional 50 vehicles parking spaces is proposed and this is welcomed.
22. The details submitted by the applicant advise that coach and minibus parking is also available on site and that this will not be increased as part of the proposal. WBC Highways officers advise that there is no objection to this as during any major events vehicle parking spaces are used by coaches and minibuses as spectators travel in larger groups.
23. The applicant's Design and Access statement also advises that during larger events there is access to other sports clubs car parks (ie Reading Cricket Club) which are a short walk from the site are available for parking. WBC Highways officers have

advised that an event parking management plan should be required by condition to include details of the number of spectators and parking arrangement to be promoted by the site's management (14).

24. The applicant has submitted a Construction Method Statement within the Design & Access Statement. This is generally acceptable; however WBC Highways officers advise that additional information relating to construction vehicle access, wheel washing facilities and street sweeping are required. As such a condition requiring the submission of an updated Construction Method Statement is included within officer's recommendation (10).

#### **Impact on Neighbours:**

25. The only residential neighbour which could potentially be impacted from the proposal is the neighbouring residential care home. Due to the separation distance of 45 metres between the proposed pitch and adjacent care home it is considered that there is no harm in respect of overlooking, loss of light and overbearing to the neighbouring site. The site is an existing sports pitch, which is used for training, and the more intensive use is considered not to result in any detriment to the neighbour amenity as a result of the proposal; as such, the proposal is considered to accord with policy CP3 in this respect.

#### **Environmental Health**

##### *Noise*

26. The development will result in an increased use of the site as demonstrated in appendix D of the applicant's Design & Access Statement. The quality of pitch combined with the proposed floodlighting would allow for greater use of the site; however WBC Environmental Health officers have advised that subject to restrictions on the hours of use (11), the proposal would not result in any harm in respect of noise to adjacent neighbours. The applicant is not proposing an amendment to the hours of use of the site, as this is already restricted by the club, but amendments are sought for the improvements to the pitch and installation of lighting, which would allow the proposed pitch to be used for longer periods of the day and throughout the year.

##### *Lighting*

27. The proposed illumination has been reviewed by WBC Environmental Health officers and is considered to acceptable in this location. There is existing lighting on the site and the proposal is seeking to remove this and install 4 lighting columns which would accommodate a total of 16 luminaires. The proposed luminaires and columns would be sited to face into the artificial grass pitch as demonstrated in Appendix A of the Design and Access Statement and Floodlighting plan 'SSSL203-06'. The lighting would not face towards the neighbouring residential care home, and the light spill towards the adjacent site is considered not to result in any harm to the amenity of the residents of the neighbouring site. As such, officers have advised that subject to a condition on the hours of use of the lighting and pitches (11) and details of baffles and/or louvres (12) the proposal would accord with Core Strategy policy CP3.

#### **Flood Risk & Drainage**

28. WBC Flood Risk & Drainage officer has raised no objection to the proposed development; however due to the introduction of an artificial turf pitch on site and increased hardstanding, the officer advises that a drainage condition should be submitted prior to commencement of development to accord with policy CC10 of the MDD Local Plan. As such, the officer has recommended a condition (9) to be attached to planning permission.

### **Sport England**

29. Sport England have been consulted on the proposed development and have raised no objection to the proposed development as it is considered to meet the exception criteria outlined within policy E5 of the Sport England adopted playing field policy.

30. The officer advises that a condition requiring the proposed pitch to be constructed in accordance with the details outlined on plan number 'SS2203 -04'. This plans and details have been conditioned (2 & 3).

### **Biodiversity**

31. WBC Biodiversity officer has advised that there is no objection to the proposed development subject to a condition on lighting hours in accordance with the details submitted by the applicant. The officer advises that although there is a degree of light spill, the lighting would not spill into areas which would cause significant disturbance or loss of light sensitive protected species. Moreover, there is already artificial light on site, and the proposal would not cause significant additional disturbance. The proposal is considered to accord with policy TB23 of the MDD Local Plan.

### **Archaeology:**

32. Berkshire Archaeology have been consulted on the proposed development due to the site's location within an area of Archaeological interest. The application site is located within an archaeological sensitive area with 7 Scheduled Ancient Monuments of national importance to the south-east and north of Charvil. One of these scheduled areas is located on the northern side of the A4 and approximately 50 metres from the proposed pitch. A further scheduled area lies to the north-east of Sonning Golf Course which is located adjacent (to the north) of the site.

33. The officer at Berkshire Archaeology advises that the *'scheduled areas include numerous crop mark features indicating extensive activity within the area, and indicate an important pre-historic monumental landscape. There is a high potential for archaeological remains to be present outside the scheduled areas as illustrated by the finds recorded during field walking surveys and archaeological investigations at East Park Farm. These excavation outside the scheduled areas identified settlement dating to the Neolithic and Bronze Age, as well as evidence for activity dating to the Palaeolithic, Roman and post-Roman periods.'*

34. The proposed development includes stripping the topsoil and therefore potential to disturb archaeological remain below. The applicant has been asked to carry out investigation works on site prior to recommendation to ensure there is no harm to archaeological remains, which could have national significance.

35. A Geophysical survey was submitted on 1<sup>st</sup> June 2017 by the applicant. The survey provided a good indication that that the site does not contain significant archaeological deposits. The Archaeology consultants advise that whilst geophysical surveys are a good guide, they do not always identify all archaeological remains; there the results of the survey will need to be ground tested with field work. A condition (15) is recommended for field work to be conducted prior to commencement of development, subject to which the proposed development is considered to accord with policy TB25 of the MDD Local Plan.

**Other**

36. Parish have advised that wider public consultation should have been carried out. In line with the Borough’s Statement of Community Involvement, the Local Planning Authority have consulted all neighbours and consultee’s on the application. A site notice was also displayed on site and an advertisement of the application was placed in the local newspaper.

**CONCLUSION**

The principle of the development is considered acceptable as it would support an established sports ground and improve facilities in sport in the local area. There is considered to be no detriment to the verdant character of the site, wider landscape character and biodiversity. In respect of neighbour amenity, highway, flood risk and archaeology conditions are suggested to mitigate any harm. As such the proposed development is considered to accord with the NPPF, Core Strategy policies CP3, CP6 and CP11 and MDD Local Plan policies CC03, CC05, CC07, TB21, TB24 and TB25.

**CONTACT DETAILS**

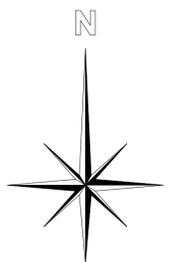
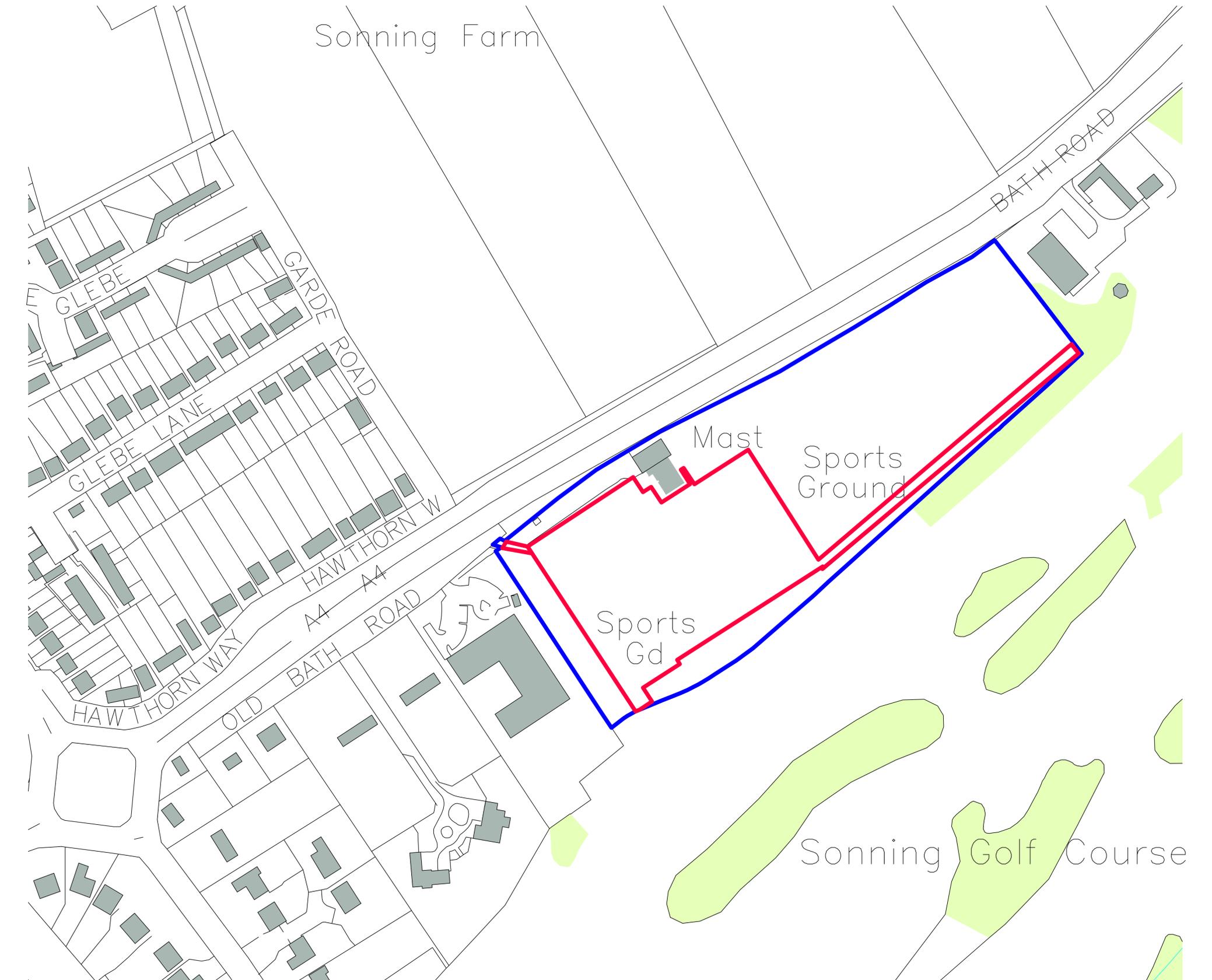
<b>Service</b>	<b>Telephone</b>	<b>Email</b>
Development Management and Regulatory Services	0118 974 6428 / 6429	development.control@wokingham.gov.uk

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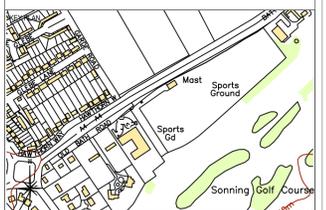
**Redingensians RFC**  
Creation of Artificial Grass Pitch (AGP)

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- NOTES
-  Application site and area required for project implementation (construction access adjoining Old Bath Road)
  -  Boundary of land under ownership of Redingensians RFC



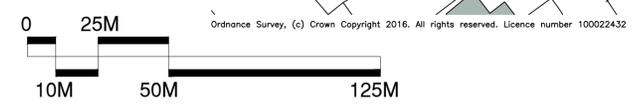
01	PLANNING ISSUE	TB	30/06/17
00	PLANNING ISSUE	SMD	16/02/17
REV	DESCRIPTION	BY	CHK DATE



AUTHOR  
Sam McDonald

TITLE  
**Redingensians RFC**  
Creation of Artificial Grass Pitch (AGP)  
Block Plan of Site

PROJECT	SCALE	SIZE
<b>SS2203</b>	1:1250	A1
DRAWING No	REVISION	
<b>02</b>	01	



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# Redingensians RFC

## Creation of Artificial Grass Pitch (AGP)

### Onsite car parking capacity

#### Existing:

- 210no. standard car parking spaces
- 4no. seventeen seat mini bus parking spaces
- 2no. 57 seat coach parking spaces

#### Future:

- 307no. standard car parking spaces
- 4no. seventeen seat mini bus parking spaces
- 2no. 57 seat coach parking spaces

### Car park management

Use parking spaces closest to Artificial Grass Pitch (AGP) during the following activities:

- Monday evening (tag/touch rugby)
- Tuesday evening (senior training)
- Wednesday evening (junior training)
- Thursday evening (senior training)
- RFU organised use (through the week)

Use parking spaces closest to and furthest from the Artificial Grass Pitch (AGP) during the following activities:

- Saturday afternoons (senior rugby - sometimes 3x matches)
- Sunday mornings (mini/junior training and matches)
- Sunday afternoons (colts/ county U/20 matches)

### Overspill parking arrangements

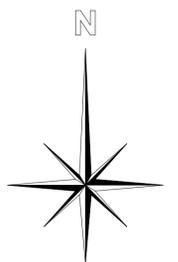
Onsite car parking is only generally problematic when the club hosts a Youth Triangular or the Mini Festival.

On these occasions, arrangements are provided for offsite parking with other local sports grounds and schools.

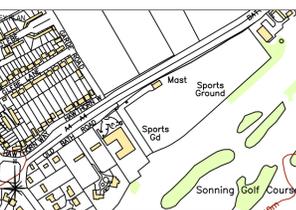
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#### NOTES

- New artificial grass pitch area
- New hard standing areas
- New maintenance equipment store 12.12m L x 2.44m W x 2.89m H
- New floodlight masts with luminaires
- New team shelters (dug outs)
- New 1.2m high Duralock pitch perimeter barrier with access gates
- New sculpted grass mound formed with excavated soils
- New scoreboard
- Existing onsite car parking area
- Proposed additional car parking area and associated access
- Existing onsite mini bus parking area
- Existing onsite 57 seated coach parking area
- Existing vehicular access driveways



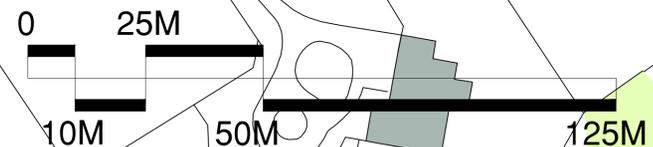
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00	PLANNING ISSUE	SMD	25/05/17
REV	DESCRIPTION	BY	CHK/ DATE



AUTHOR  
 Sam McDonald

TITLE  
**Redingensians RFC**  
 Creation of Artificial Grass Pitch (AGP)  
 Existing and Proposed Parking

PROJECT	SCALE	SIZE
SS2203	1:750	A1
DRAWING No	REVISION	
09	01	



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# Redingensians RFC

## Creation of Artificial Grass Pitch (AGP)

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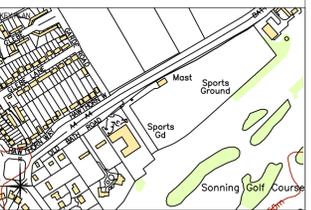
- NOTES
- New artificial grass pitch area
  - New hard standing areas
  - New maintenance equipment store  
12.12m L x 2.44m W x 2.89m H
  - New floodlight masts with luminaires
  - New team shelters (dug outs)
  - New 1.2m high Duralock pitch perimeter barrier with access gates
  - New sculpted grass mound formed with excavated soils
  - New scoreboard



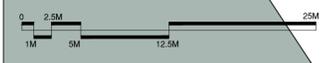
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00	PLANNING ISSUE	SMD	16/02/17
REV	DESCRIPTION	BY	CHK DATE



AUTHOR		Sam McDonald	
TITLE			
Redingensians RFC Creation of Artificial Grass Pitch (AGP) Proposed AGP Plan			
PROJECT	SCALE	SIZE	
SS2203	1:300	A1	
DRAWING No			REVISION
04			00



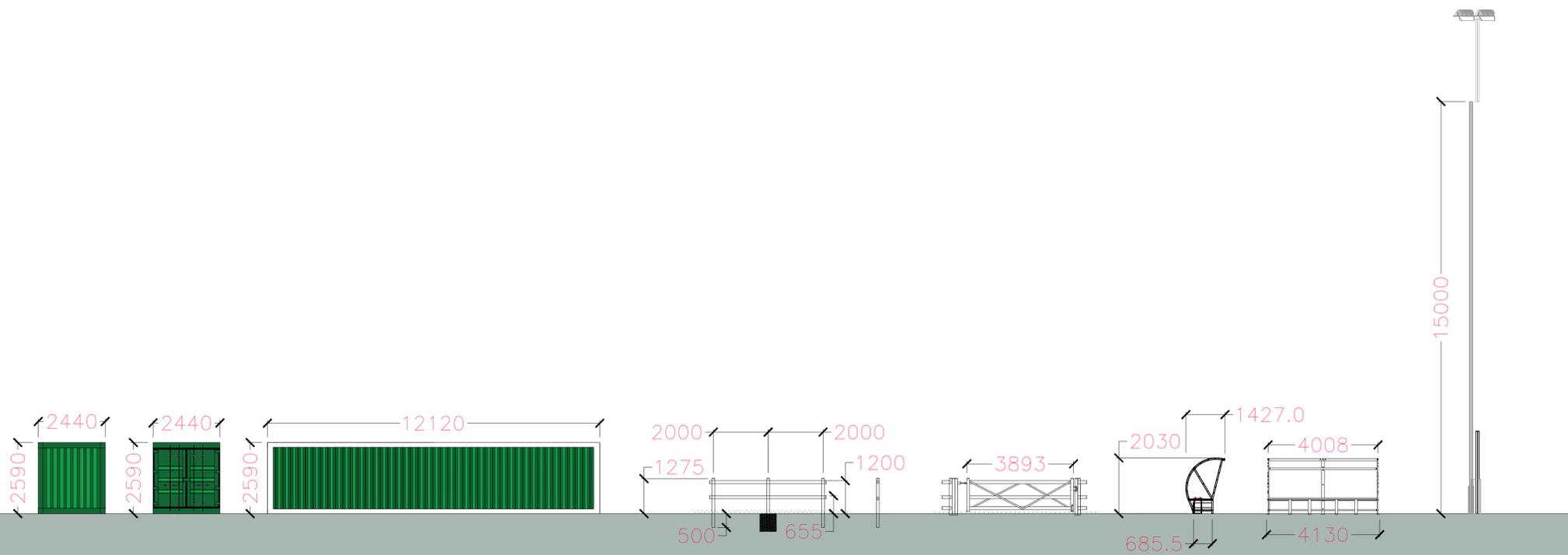
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# Redingensians RFC

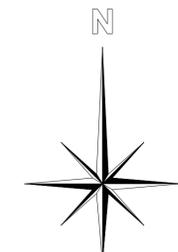
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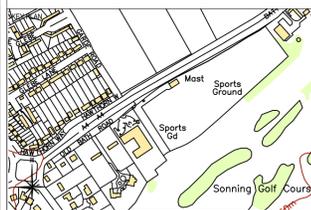
- NOTES
- New artificial grass pitch area
  - New hard standing areas
  - New maintenance equipment store 12.12m L x 2.44m W x 2.89m H
  - New floodlight masts with luminaires
  - New team shelters (dug outs)
  - New 1.2m high Duralock pitch perimeter barrier with access gates
  - New sculpted grass mound formed with excavated soils
  - New scoreboard



Maintenance equipment store back    Maintenance equipment store front    Maintenance equipment store both sides    Pitch perimeter barrier    Access gate to pitch perimeter barrier    Team shelter (dug out)    15m high floodlight with luminaires



01	PLANNING ISSUE	SMD	25/05/17
00	PLANNING ISSUE	SMD	16/02/17
REV	DESCRIPTION	BY	CHK DATE



AUTHOR  
Sam McDonald

TITLE  
**Redingensians RFC**  
Creation of Artificial Grass Pitch (AGP)  
Proposed Elevations

PROJECT	SCALE	SIZE
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DRAWING NO	REVISION	
<b>05</b>	01	

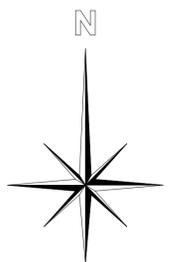
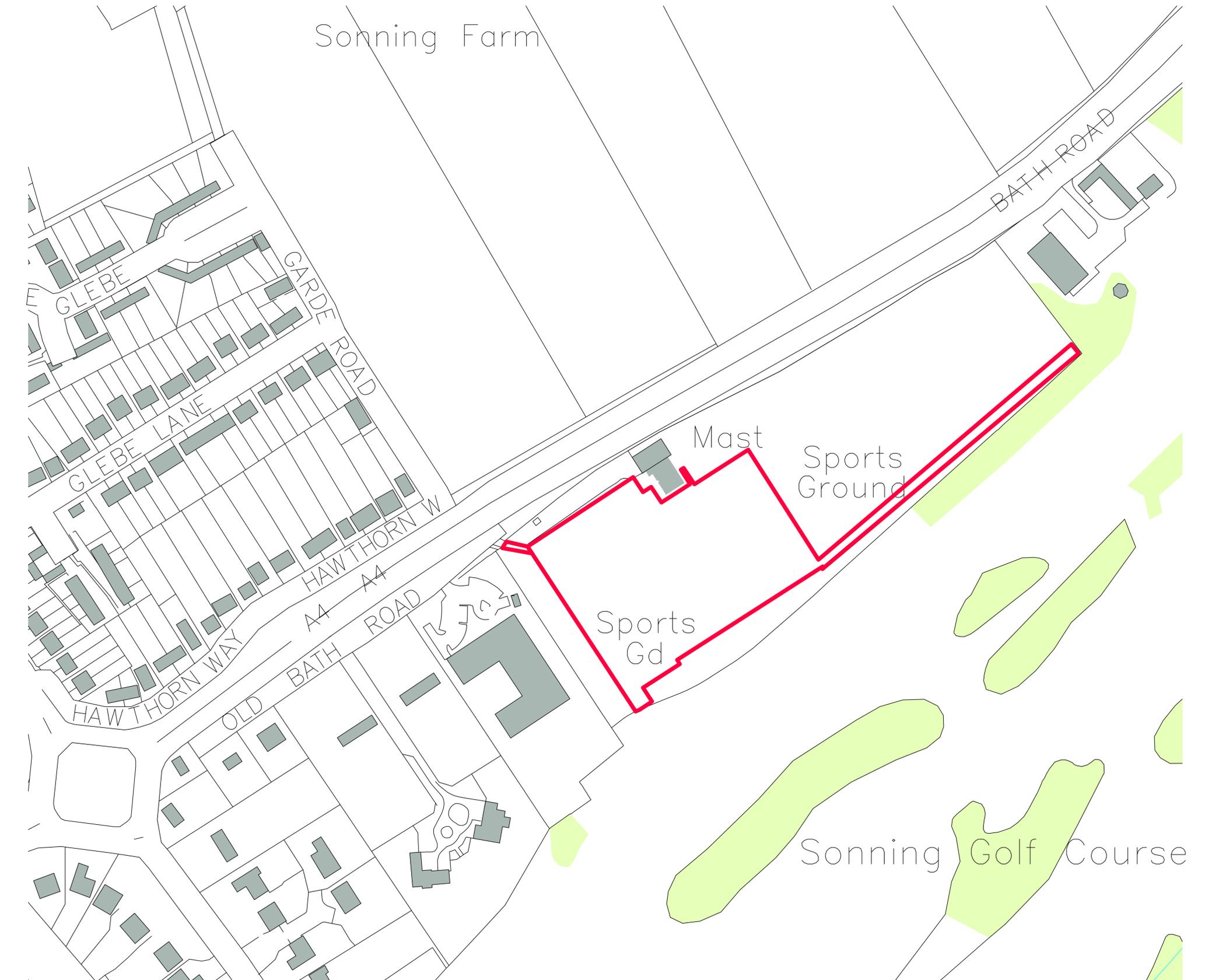


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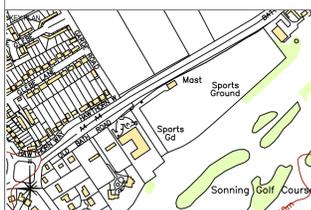
**Redingensians RFC**  
Creation of Artificial Grass Pitch (AGP)

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NOTES  
Application site and area required for project implementation (construction access adjoining Old Bath Road)



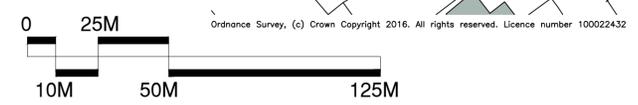
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REV	DESCRIPTION	BY	CHK/ DATE



AUTHOR  
Sam McDonald

TITLE  
**Redingensians RFC**  
Creation of Artificial Grass Pitch (AGP)  
Site Location Plan

PROJECT	SCALE	SIZE
<b>SS2203</b>	1:1250	A1
DRAWING No	REVISION	
<b>01</b>	<b>01</b>	



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WOKINGHAM BOROUGH COUNCIL  
PARISH COUNCIL OBSERVATION SHEET

PLANNING APPLICATION.

APPLICATION No. 170580

Officer POOJA KUMAR

Parish SONNING.

Proposal INSTALLATION OF ARTIFICIAL GRASS PITCH (AGP) TO FORM A FULL SIZED PLAYING ENCLOSURE MEASURING 122M X 80M WITH ASSOCIATED TECHNICAL AREAS. INSTALLATION OF NEW PITCH PERIMETER AND ASSOCIATED GATED ENTRANCE TO FORM A PLAYING ENCLOSURE, NEW HARDSTANDING, NEW (ARTIFICIAL) FLOOD LIGHTING., NEW MAINTENANCE EQUIPMENT STORE AND RETENTION OF SOIL ARISING ONSITE TO FORM A SCULPTURED GRASS MOUND TO AN ADJACENT GRASS PITCH.

Address OLD REDINGEMSIANS SPORTS GROUND

Applicant MR ROSS BAXTER.

---

Sonning Parish Council has carefully considered this application and wish to raise the following concerns:

- This is a significant upgrade and could have an impact on a much wider area than has been considered, therefore a fuller public consultation should be undertaken.
- The improvements will make this a very attractive venue, increasing the usage and improving its standing in the sports world. This could result in the Club hosting higher profile matches and is bound to attract more supporters to the location and potentially swamping the area with additional vehicles.
- There is a comprehensive transport plan for public transport but this cannot be guaranteed. Many supporters are likely to use their own vehicles. This already happens with vehicles parking on adjacent residential roads.
- The floodlighting is of concerns due to its proximity to Sunrise, the adjoining home for the elderly. The lighting, while not flooding directly into the windows, creates a glow in the night sky which could cause disturbance and a suggested deadline of 9.30 pm might go some way to reducing this.
- The Council fully supports the principle of an all-weather facility however they recommend refusal of the application as it stands and until the above concerns are addressed.

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